

ZONING CHANGE REVIEW SHEET

CASE: C814-06-0218 – Lakeline Station PUD **P.C. DATE:** August 14, 2007

ADDRESS: North FM 620 Road and 13713 Rutledge Spur

OWNERS: PCG Summit – Lakeline Station L.P. (Steve Levenson)

AGENT: Armbrust and Brown, L.L.P. (David Armbrust)

ZONING FROM: I-RR **TO:** PUD

AREA: 326.798 acres

SUMMARY STAFF RECOMMENDATION:

The Staff recommendation is to grant planned unit development (PUD) district zoning for Lakeline Station PUD, as illustrated in Exhibits B through I, with the environmental and transportation conditions set forth in Exhibits B, B-1 and B-2.

PLANNING COMMISSION RECOMMENDATION:

August 14, 2007: *APPROVED APPLICANT'S REQUEST FOR PUD DISTRICT ZONING.*

[M. DEALEY, J. REDDY 2ND] (7-0) C. GALINDO – ABSENT

EXHIBITS TO STAFF REPORT:

Exhibits A, A-1, A-2:	Vicinity Map, Zoning Map and Aerial View
Exhibit B:	Support Material for the Environmental Board
Exhibit B-1:	Public Works Department Memo on Lakeline Boulevard Paired Couplet
Exhibit B-2:	Transportation Review Staff Memo
Exhibit C:	Land Use Plan
Exhibit D:	Site Development Regulations
Exhibit E:	Zoning Use Summary Table
Exhibit F:	Street Standards and Parking Regulations
Exhibit G:	Street Cross Section Illustrations
Exhibit H:	Environmental Feature and Critical Environmental Feature Maps
Exhibit I:	Environmental and Water Quality Regulations
Exhibit J:	Code Modifications Table

ISSUES:

The Applicant is in agreement with the Planning Commission recommendation.

The Applicant is in agreement with the Staff recommendation except for the following three items:

- 1) That the PUD incorporate 30 percent open space. The Applicant is proposing 20 percent. Please refer to Exhibit B.
- 2) That the Lakeline Boulevard couplet have a design speed of 45 miles per hour. The Applicant is proposing a 30-mile per hour design speed to promote a pedestrian – oriented community and land uses. Please refer to Exhibit B-2.
- 3) That the portion of Lakeline Boulevard designed as a couplet have 10-foot wide maneuver lanes adjacent to parallel parking. Please refer to Exhibit B-2.

The Lakeline Station PUD was considered by the Environmental Board at their August 1, 2007 meeting. Although there was a 5-1 vote to deny the PUD, there was an initial motion with a second to support the Staff recommendation, including a requirement to provide 30 percent open space.

The Applicant has met with representatives of Avery Ranch and Capital Metro to discuss the PUD.

DEPARTMENT COMMENTS:

The subject property consists of undeveloped and agricultural land, and was annexed into the City limits in December 1994 as interim-rural residence (I-RR) zoning. Please refer to Exhibits A, A-1 and A-2. The surrounding area consists of existing single family residences and commercial development under construction within the Avery Ranch PUD to the north; the Round Rock ISD Sports Complex, multi-family and undeveloped land to the east; research and development businesses and a greenbelt to the south along Spectrum Drive, single family residences along Rutledge Spur, and undeveloped land and the Capital Metro Northwest Park and Ride facility to the west within the Leander Rehabilitation PUD. The Southern Pacific Railroad forms the western property line and Lakeline Boulevard extends through the southern portion of the property. Rutledge Spur terminates at the southern end of the property and is not contemplated to be used as an access point.

PUD Land Use Plan

The Applicant has requested planned unit development (PUD) zoning so that the property may be developed with mixed-use development that provides for a range of residential, commercial, civic and open spaces integrated into a master planned community. The PUD Land Use Plan contains five development areas: 1) a *Transit Oriented Development (TOD) Mixed Use District*; 2) *Attached / Cluster Residential / Mixed Use District*; 3) *the Single Family Residential / Mixed Use District*; 4) *Civic District*; and 5) *Parks / Open Space / Detention*.

The *TOD Mixed Use District* is generally located adjacent to the existing Capital Metro Northwest Park and Ride facility, provides for a mixture of commercial and residential uses and up to 711 residences. The *Attached Cluster Residential / Mixed Use District* is generally located along both sides of Lakeline Boulevard and north of the western 17 acre park area and allows for a greater range of residential product types. Commercial uses are allowed in this district but must be incorporated into live-work units. The *Single*

Family Residential / Mixed Use District is located adjacent to the Avery Ranch PUD and extends to the western property line which abuts the Round Rock ISD Sports Complex. This district permits a mix of residential designs alongside conventional single family and townhome residential uses. Like the *Attached Cluster Residential / Mixed Use District*, commercial uses are allowed in this district but must be incorporated into live-work units. The *Civic District* is approximately 12.5 acres and is adjacent to the Sports Complex. Its permitted uses include art gallery, community recreation and public or private educational facilities. Lakeline Station PUD incorporates a total of 63 acres of *Parks / Open Space / Detention*, in the form of interconnected greenways and pocket parks spread throughout the development, as well as a comprehensive network of trails and sidewalks to encourage pedestrian circulation and recreation opportunities. Integrated together, these uses will contribute to a development that promotes living, working, shopping and recreation opportunities within the same development. Please refer to Exhibit C.

Residential uses account for a maximum of 2,776 dwelling units. The different housing types are designed to be clustered together in order to preserve open space and achieve water quality benefits. The developer has committed to have 10 percent of the residential units serve households at 80 percent median family income. In addition, the developer has also committed to participate in the City's Green Building program, at a two Star rating, as included in Exhibit I.

Lakeline Station PUD provides a floor-to-area ratio of 6 to 1 in the *TOD Mixed Use District*, 3 to 1 in the *Attached Cluster Residential / Mixed Use District* and 2 to 1 in the *Single Family Residential / Mixed Use District*.

There are various building types allowed within the PUD and each has their own set of site development regulations, as described in Exhibit D. Building types included within the PUD are as follows:

- Commercial / mixed use buildings – Ground floor retail with office space above or multi-story office buildings. Parking is concealed behind buildings and located within structures that are not fully visible from the sidewalk, or placed along less-traveled streets.
- Embedded-Garage Multifamily – A block-based building with an internal parking garage that may also include ground level retail uses.
- Podium – Single-level concrete structure of parking with wrap-around buildings. Building entries must address the street. Ground level retail is permitted.
- Tuck-Under – A medium-density multifamily type structure with attached parking accessed from alleys or private drives. Ground level retail is permitted.
- Lakeline Townhome – Townhomes with individual ground-floor entries addressing the street. Garages are alley-accessed.
- Townhome Court – Clusters of 12 to 15 attached two to three-story townhomes that face onto three sides of a shared green space.
- Live-Work Townhomes – Residential-office units with the work space addressing the street and alley-accessed garages.

- Green Court / Paseo Court – Clusters of 6 to 15 detached single family homes that face onto three sides of a shared green space.
- Lakeline Small Lot – Small lot, detached single family residences built near neighborhood amenities. Front-loaded parking is not permitted.
- Lakeline Standard Lot – Standard size, detached single family residences with front entries and outdoor spaces visible from the street.
- Estate Lot – Detached single family residences with a lot size of at least 7,000 square feet.

Transportation Network

The transportation network within Lakeline Station PUD is proposed to consist of Lakeline Boulevard, designed as two separate one-way streets (a “couplet”), connector streets, local streets and alleys for automobiles, as well as trails and sidewalks for pedestrians and bicyclists. Connections are proposed to South Canoa Hills Trail within the Avery Ranch PUD, an at-grade crossing into the Leander Rehabilitation PUD, the existing Lakeline Boulevard and the future extension of Lakeline Mall Drive. A connection to Rutledge Spur is not proposed. The street standards have been designed to maintain efficient traffic circulation while promoting pedestrian use. Narrower street widths will assist in slowing vehicle speeds, and encourage walking and bicycling within the neighborhood. Cul-de-sacs are strongly discouraged in order to achieve an interconnected street system and further the objective of a mixed use, pedestrian-oriented development. Please refer to Exhibits F and G.

Parks, Open Space and Detention Areas

The northeast portion of Lakeline Station PUD is adjacent to the Round Rock ISD Sports Complex. The Applicant is providing 63.5 acres of parks, open space and detention areas. While these parks and open spaces will be open to public use, they will be privately maintained.

Drainage and Water Quality

The Applicant proposes to provide wet ponds which provide a more efficient method of removing pollutants compared to that required by City Code. In addition, the water quality ponds will be constructed as an amenity/recreation area to enhance the community while serving a vital water quality function. Water quality ponds will capture approximately twenty percent (20%) greater volume than is required by City Code, as further described in Exhibit I. The Applicant will also capture and isolate rooftop drainage from the commercial areas and use it to irrigate park and landscape areas.

Code Modifications Requested with the PUD

The Code Modifications requested with the Lakeline Station PUD cover the following areas: contiguous zoning boundaries; multifamily residence floor-to-area ratio; front and side yard setbacks; number of curb cuts / driveways; open space requirements; the housing affordability analysis; compatibility standards; screening of specific land uses; commercial design standards; lots abutting dedicated public streets; streets, alleys and pedestrian paths; design and construction of streets; arterial streets; collector streets; standards for design, construction, alteration or repair of sidewalks, driveway approaches,

pavement, appurtenances on public property; existing driveway requirements; off-street parking and loading; minimum parking and loading requirements for motor vehicles and bicycles; roadway clearing limits; cut and fill requirements, impervious cover limits and subdivision identification signs. In general, the Code Modifications allow for a compact, pedestrian-scaled, mixed-use community to be developed, promotes the interaction between residential and non-residential uses, and provides opportunities for the development of neighborhood and community-serving businesses and activities. The requested Code Modifications are specifically defined in Exhibit J.

Staff recommends PUD zoning for Lakeline Station, given that: 1) it provides for a mix of residential, live-work and commercial land uses and includes a range of housing types which will serve to diversify residential options in the area; 2) housing types will be clustered together and street widths narrowed to preserve open space, slow vehicle speeds, and reduce impervious cover which in turn benefits water quality; 3) it has a civic and park / open space component that will provide recreational amenities; 4) it provides a *TOD Mixed Use District* that will increase the population density in close proximity to the future rail line; 5) it will provide for different modes of transportation through a system of inter-connected streets, as well as sidewalks and trails to serve pedestrians and bicyclists, all of which benefit circulation within the property; 6) it includes an affordable housing component; 7) the development will achieve a two-Star rating in the Austin Energy Green Building Program; 8) wet ponds, rainwater harvesting and other water conservation strategies will be incorporated into the development to benefit water quality and reduce consumption; 9) an Integrated Pest Management Plan (IPM) will be utilized in site development; and 10) enhanced protection of critical environmental features is provided.

EXISTING ZONING AND LAND USES:

	ZONING	LAND USES
<i>Site</i>	I-RR	Undeveloped and Agricultural
<i>North</i>	PUD	Single family residences within Avery Ranch PUD
<i>South</i>	PUD; R & D; IP-PDA; P; CS; I-RR	Undeveloped (planned for multi-family and office in the Leander Rehabilitation PUD); Undeveloped; Semiconductor manufacturing; Office – warehousing; Greenbelt; Single family residences
<i>East</i>	PUD; IP-PDA; MF-1; LR; MF-3-CO	Bank, Restaurant and commercial uses under construction; Round Rock ISD Sports Complex; Multi-family; Undeveloped
<i>West</i>	PUD	Undeveloped (planned for single family, multi-family and commercial) and Capital Metro Northwest Park and Ride facility within the Leander Rehabilitation PUD.

TOD DISTRICT: Northwest Park and Ride **TIA:** Is not required – within the area covered by Senate Bill 1396

WATERSHEDS: Lake Creek and South Brushy Creek **DESIRED DEVELOPMENT ZONE:** Yes

CAPITOL VIEW CORRIDOR: No **SCENIC ROADWAY:** No

NEIGHBORHOOD ORGANIZATIONS:

485 – Riviera Springs Community Development Association
 604 – Davis Spring HOA
 690 – Avery Ranch Owners Association, Inc.
 701 – Avery Ranch Neighborhood Association

SCHOOLS:

This property is within the Round Rock Independent School District.

CASE HISTORIES:

NUMBER	REQUEST	COMMISSION	CITY COUNCIL
C14-99-0090 – Davis Springs	R & D to LI	To Grant IP for Tract 1 and IP-CO for Tract 2	Approved Commission's recommendation with conditions (8-26-99).
C814-99-0001.02 – Avery Ranch PUD Amendment #2	An amendment to change conditions of zoning	To Grant changes to impervious cover limits and allow laundry service as a permitted use, limited to 4,250 square feet.	Approved Commission's recommendation (4-24-03).
C814-97-0001 – Leander Rehabilitation Site	I-RR to PUD	To Grant PUD with conditions	Denied PUD zoning (11-20-97). <u>Note:</u> By Orders of the Special Board of Review, the State overrode the City Council's action and approved PUD zoning (7-1-99).
C14-99-0023 – UDR Parmer Lane West	LR; MF-1 to GR; MF-3-CO	To Grant MF-3-CO with CO limit of 55% impervious cover	LR tract withdrawn; Approved MF-3-CO with conditions for Tract 2 (4-15-99).
C14-95-0093 – Davis Spring	SF-2 to IP	To Grant LO for the first 100 feet; IP-PDA for the remainder	Approved LO and IP-PDA subject to conditions (12-14-95).
C14-93-0107 – Davis Spring	R & D to R & D-PDA	To Grant IP-PDA with conditions	Approved IP-PDA with conditions (11-4-

Commercial, Tract 1			93).
C14-93-0081 – Davis Spring Properties, Tract 1	NO; P; R & D to MF-3	To Grant MF-3-CO with density limited to 24 units per acre	Approved MF-3-CO as Commission recommended (9-2-93).

RELATED CASES:

The subject property was annexed into the City limits on December 6, 1994. There are no related subdivision or site plan cases on the subject property.

ABUTTING STREETS:

NAME	ROW	PAVEMENT	CLASSIFICATION
Lakeline Boulevard (west of the site)	110 feet	2 lanes at 40 feet	Four-lane divided Major Arterial
South Canoa Hills Trail	60 feet	40 feet	Residential Collector
Dunham Forest	50 feet	30 feet	Local Street
Rutledge Spur / County Road 183	Approximately 50 feet	10 to 12 feet	Local Street with substandard width

CITY COUNCIL DATE: October 11, 2007**ACTION:****ORDINANCE READINGS:** 1st2nd3rd**ORDINANCE NUMBER:**

CASE MANAGER: Wendy Rhoades
e-mail: wendy.rhoades@ci.austin.tx.us

PHONE: 974-7719

SUMMARY STAFF RECOMMENDATION:

The Staff recommendation is to grant planned unit development (PUD) district zoning for Lakeline Station PUD, as illustrated in Exhibits B through I, with the environmental and transportation conditions set forth in Exhibits B, B-1 and B-2.

BASIS FOR LAND USE RECOMMENDATION (ZONING PRINCIPLES)

1. The proposed zoning should be consistent with the purpose statement of the district sought.

The Planned Unit Development (PUD) zoning district is intended for large or complex developments under unified control, planned as a single contiguous project. The PUD is intended to allow single or multi-use projects within its boundaries and provides greater design flexibility for development proposed within the PUD. Use of the PUD district should result in development superior to that which would occur using conventional zoning and subdivision regulations. PUD zoning is appropriate if the development enhances preservation of the natural environment; encourages high quality development and innovative design; and ensures adequate public facilities and services for development within the PUD.

2. Zoning changes should promote an orderly and compatible relationship among land uses.

Staff recommends PUD zoning for Lakeline Station, given that: 1) it provides for a mix of residential, live-work and commercial land uses and includes a range of housing types which will serve to diversify residential options in the area; 2) housing types will be clustered together and street widths narrowed to preserve open space, slow vehicle speeds, and reduce impervious cover which in turn benefits water quality; 3) it has a civic and park / open space component that will provide recreational amenities; 4) it provides a *TOD Mixed Use District* that will increase the population density in close proximity to the future rail line; 5) it will provide for different modes of transportation through a system of inter-connected streets, as well as sidewalks and trails to serve pedestrians and bicyclists, all of which benefit circulation within the property; 6) it includes an affordable housing component; 7) the development will achieve a two-Star rating in the Austin Energy Green Building Program; 8) wet ponds, rainwater harvesting and other water conservation strategies will be incorporated into the development to benefit water quality and reduce consumption; 9) an Integrated Pest Management Plan (IPM) will be utilized in site development; and 10) enhanced protection of critical environmental features is provided.

EXISTING CONDITIONS

Site Characteristics

The property consists of former ranchland and residential structures associated with the ranch. The site is relatively flat with gentle slopes to the north and southeast. The southern portion of the site drains towards the southeast, to Davis Spring. The northern portion of the site drains towards the north, to two unnamed tributaries of South Brushy Creek. There is no 100-year flood plain within this site. At the northwest corner of the property there is a Critical Water Quality Zone and Water Quality Transition Zone associated with the intermittent tributary.

Impervious Cover

The maximum impervious cover proposed by the Lakeline Station PUD is 62% of the net site area.

For the *Transit Oriented Development (TOD) Mixed Use District*, the maximum impervious cover is 79.1%.

For the *Attached / Cluster Residential / Mixed Use District*, the maximum impervious cover is 84.8%.

For the *Single Family Residential / Mixed Use District*, the maximum impervious cover is 73.0%.

For the *Civic District* the maximum impervious cover is 48.8%.

For the *Parks / Open Space / Detention District*, the maximum impervious cover is 5.8%.

Therefore, *individual lots* may have a maximum allowable impervious cover that *exceeds* that allowed for the Property or development district, so long as the Owner complies with maximum impervious cover for each of the Development Districts. The Owner shall submit a Preliminary Plan for the City's review and approval which covers the entire PUD and describes the apportionment of impervious cover for each Development District, as well as that allocated for each individual lot. This Preliminary Plan shall be used as a tracking mechanism for the assignment of impervious cover.

Environmental

Please refer to Exhibits B, H and I.

Transportation

Please to refer to Exhibits B-1 and B-2.

The site is within the area covered by State legislation (S.B. 1396), which went into effect September 1, 1995. Under this legislation, the City may not "deny, limit, delay, or condition the use of development of land...because of traffic or traffic operations that would result from the proposed use or development of the land." A traffic impact analysis is not required for any development within this area, and traffic issues may not

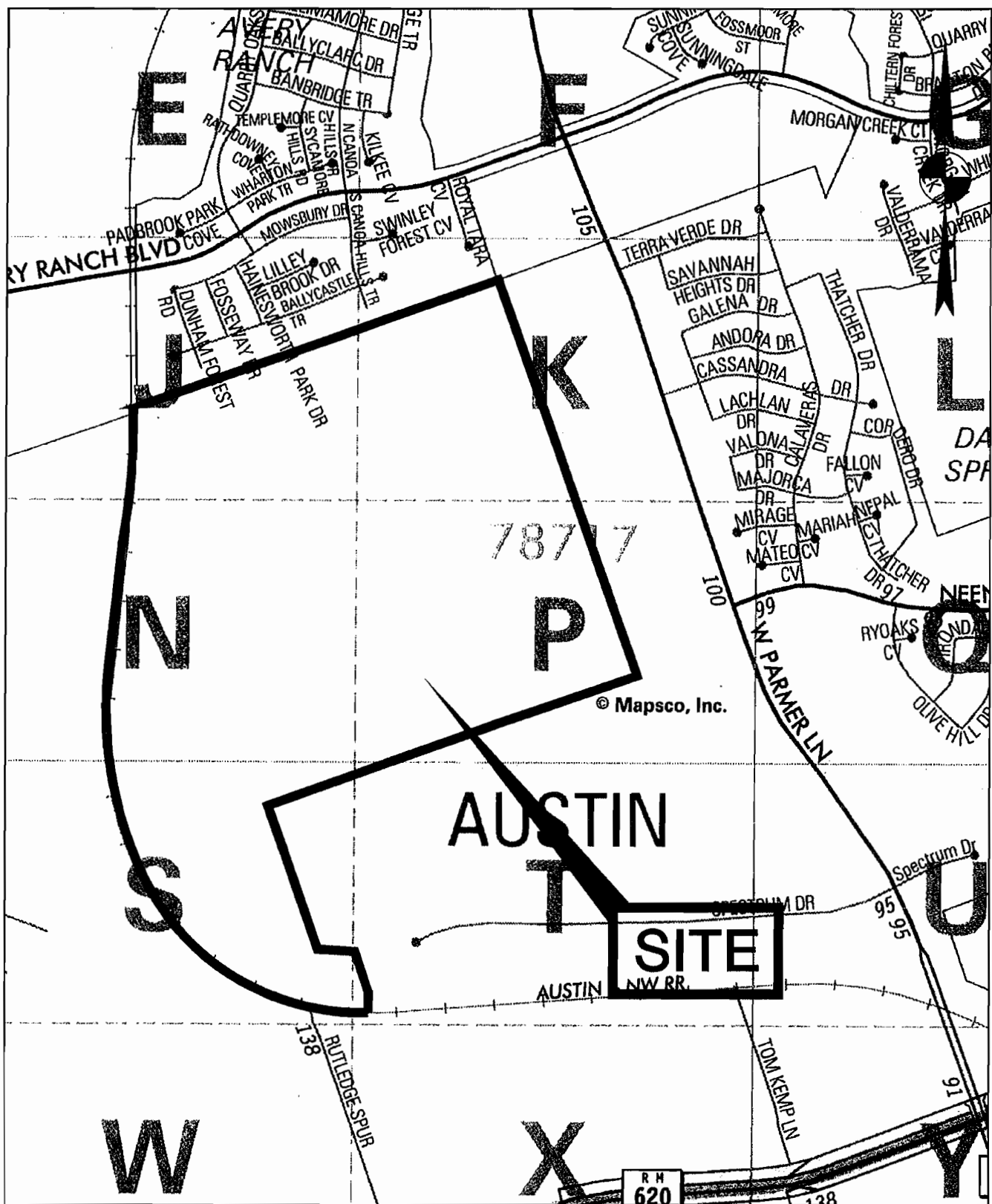
be considered in the approval of the application. Right-of-way dedication, however, may still be required.

Water and Wastewater

The landowner intends to serve each lot with City of Austin water and wastewater utilities. The landowner, at own expense, will be responsible for providing the water and wastewater utility improvements, offsite main extension, and system upgrades to serve the site and land use. The water and wastewater utility plan must be reviewed and approved by the Austin Water Utility. The plan must be in accordance with the City of Austin design criteria. The water and wastewater utility construction must be inspected by the City. The landowner must pay the associated and applicable City fees.

In order to obtain City water and wastewater utility service, the landowner must obtain City approval of a Service Extension Request. For more information pertaining to the process and submittal requirements, contact Phillip Jaeger, Austin Water Utility, 625 East 10th Street 5th Floor Waller Creek Center. The Austin Water Utility reserves the right to make additional comments and to establish other requirements with the Service extension Request, preliminary plan, subdivision plat, and water and wastewater utility plan review.

Onsite and offsite easements will be required. The subject landowner will be responsible for obtaining and providing the offsite easements and must grant all necessary onsite easements. The easements must be for location defined by the Austin Water Utility. The easements dedication must be in a form accepted by the Austin Water Utility.



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**LAKELINE STATION PUD
PARMER LANE AND RM 620**

PACIFIC SUMMIT PARTNERS

**Exhibit A
SITE LOCATION MAP**

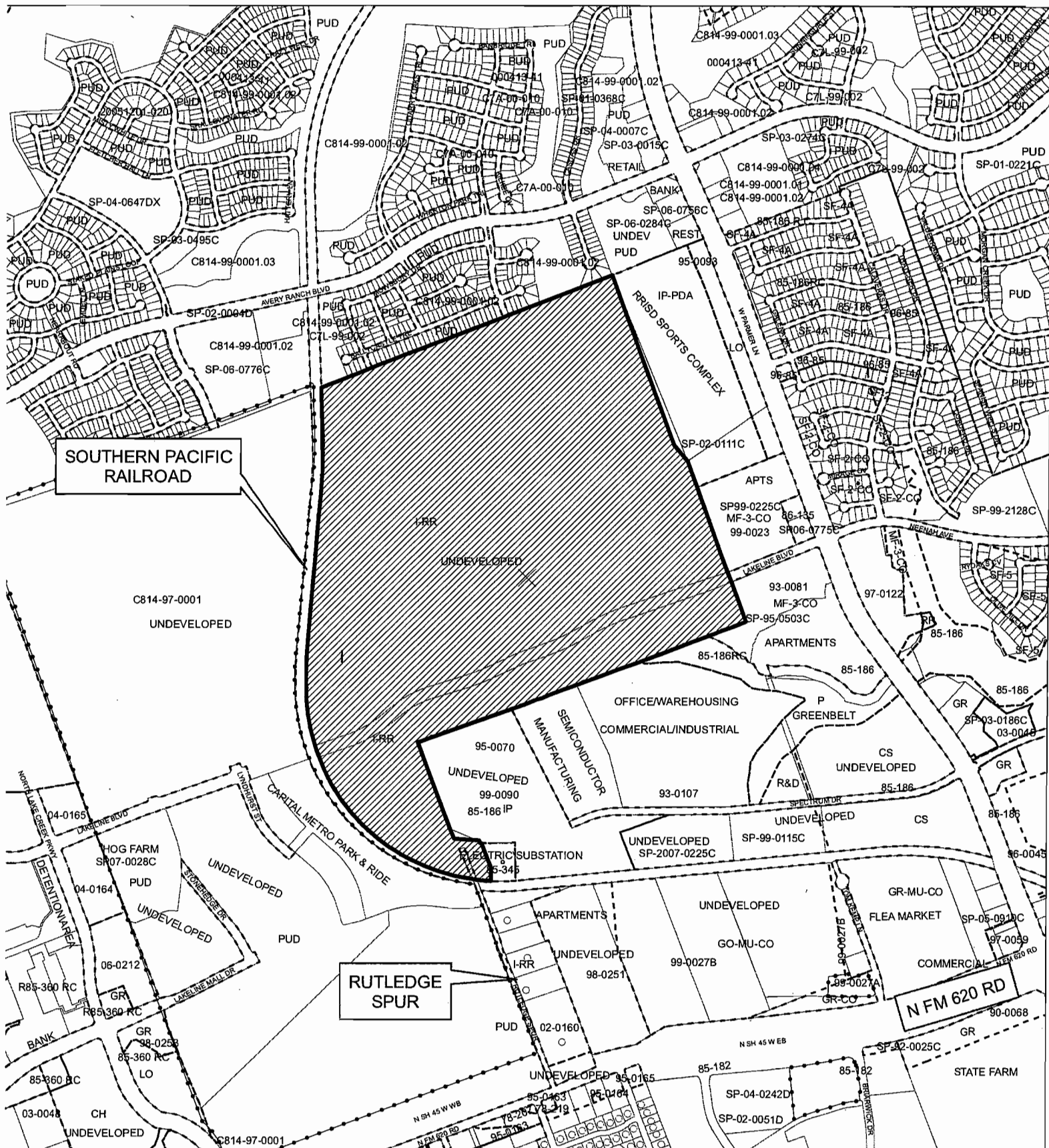
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SCALE: N.T.S.

DRAWN BY: PFQ

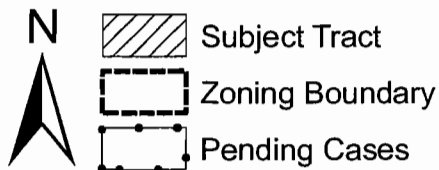
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PROJECT No.: 1655-07.09



PLANNED UNIT DEVELOPMENT
ZONING CASE#: C814-06-0218
ADDRESS: N FM 620 RD AND
13713 RUTLEDGE SPUR
SUBJECT AREA: 326.798 ACRES
GRID: G41
MANAGER: W. WALSH

EXHIBIT A-1



1" = 1200' OPERATOR: S MEEKS

This map has been produced by G.I.S. Services for the sole purpose of geographic reference. No warranty is made by the City of Austin regarding specific accuracy or completeness.



0814-08-0218

OF	1
SHEET	1
PLOTTING SCALE: 1" = 1'	
DATE PLOTTED: 03/20/07	
FILE: 15851815712061105-04d.dwg	
DRAWN BY: JJA/PJC	
DESIGNED BY: SES	
REVIEWED BY: SES	
PROJECT NO: 1655-07.00	

LAKELINE STATION PUD
PACIFIC SUMMIT PARTNERS

AERIAL EXHIBIT

Bury+Partners
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 Lincoln, Texas 78701
 Tel: (512) 770-9000 Fax: (512) 770-4005
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EXHIBIT A-2



ENVIRONMENTAL BOARD MOTION 080107-B2

Date: August 1, 2007

Subject: Lake line Station Area Plan/Planned Unit Development (PUD) C814-06-0218

Motioned By: John Dupnik, P. G.

Seconded by: John Beall

Recommendation

The Environmental Board recommends **disapproval** of a request to create a Planned Unit Development at the Lakeline Station Area Plan/Planned Unit Development (PUD)

Rationale

The Environmental Board commends the applicant for the care taken in developing this Transit Oriented Development (TOD), and for the degree to which environmental issues have been considered. The Board feels that promoting mass transit – particularly rail – is in the long-term best interest of citizens and the environment, and encourages the Applicant to continue to work with Staff until all issues have been resolved.

However, at this time the Applicant appears unable to satisfy all of Staff's recommended conditions, specifically the conditions to achieve the 30% open space to create more density and offset impervious cover. The Board realizes that clustered development around transit is good, but must be balanced with sustainable watershed health, and the amount of impervious cover proposed by the Applicant does not promote a healthy watershed.

Vote 5-1-0-2


For:

Against: Anderson, Maxwell, Moncada, Neely, Beall and Dupnik.

Abstain: Ahart

Absent: Moncada and Curra

Approved By:


Dave Anderson P.E., CFM P.E.
Environmental Board Chair



MEMORANDUM

TO: David Sullivan, Chairperson
Members of the Planning Commission

FROM: Patricia Foran, Environmental Review Specialist Senior
Watershed Protection and Development Review Department

DATE: July 11, 2007

SUBJECT: Lakeline Station Area Plan/PUD/C814-06-0218
N. Farm-to-Market Road 620

The applicant is proposing a zoning change from Interim – Rural Residence (I-RR) to Planned Unit Development (PUD) for 326.9 acres of land.

The PUD proposal consists of a high density mixed use development with five types of "development districts": Transit-oriented Development Mixed Use, Attached/Cluster Residential Mixed Use, Single Family Residential Mixed Use, Civic, and Parks/Open Space/Detention. In total, impervious cover is proposed at 62% net site area, which is approximately 196.5 acres of impervious cover. The applicant is allowed approximately 160.5 acres of impervious per Land Development Code Section 25-8-394.

The applicant is requesting six exceptions to environmental regulations. Please note that one of these exceptions (to LDC 25-8-64) is based on a staff condition.

Description of Property

The proposed PUD is situated in the South Brushy and Lake Creek Watersheds, both of which are classified as Suburban. The tract lies in the Desired Development Zone and is located over the northern portion of the Edwards Aquifer Recharge Zone. Critical Water Quality Zone (CWQZ), Water Quality Transition Zone (WQTZ), and critical environmental features (CEFs) occur within the property lines.

The existing site consists of former ranchland and residential structures associated with the ranch. Much of the terrain appears to have disturbed by clearing and rock removal activities associated with ranching activities. The site is bounded by Southern Pacific Railroad tracks on the west, residential development on the north, a stadium and

EXHIBIT B

A wetland is associated with another pond in the northeast corner. Either a modified setback of 50' from the pond's edge and 50' from the centerline of the two draws that feed it or the standard 150' setback around the ponded wetland is required.

Two geologic CEFs have also been identified as requiring protection. A collapsed sinkhole approximately 91'x29'x1-2' was identified. A buffer zone of 150' is required. A 15x12.5'x2.3' sinkhole was identified in the drainageway above a stocktank. A buffer of 300' upslope, 150' south, and 50' north is required.

Water/Wastewater

The applicant proposes to utilize City of Austin water and wastewater services.

Environmental Exception Requests

The environmental exceptions requested for this project are to LDC Sections:

1. Exception from LDC 25-2-1006(C) (Visual Screening)

"The Environmental Criteria Manual shall prescribe standards for screening in accordance with this subsection.

- (1) For a townhouse, condominium, multiple family, group, or mobile home residential use, screening is required at a property line that adjoins a residential district in which the use is not a permitted use.
- (2) For a commercial or industrial use, screening is required at a property line that adjoins a residential district.
- (3) For a civic use, screening is required at a property line that adjoins a more restrictive district in which the use is not permitted."

The applicant is requesting to remove this requirement.

2. Exception from LDC 25-8-64 (Impervious Cover Assumptions)

- "(A) This section applies to impervious cover calculations for duplex or single-family lots.
- (B) Except as provided in Subsection (C):
- (1) for each lot greater than three acres in size, 10,000 square feet of impervious cover is assumed;
 - (2) for each lot greater than one acre and not more than three acres in size, 7,000 square feet of impervious cover is assumed;

5. Exception from LDC 25-8-342 (Fill Requirements)

"Fill on a tract of land may not exceed 4' of depth."

The applicant is requesting a modification to allow fill up to 20' for areas associated with the detention/water quality facilities (this would otherwise be an administrative variance); fill up to 10' in multi-family/commercial areas; and fill up to 6' in residential areas.

6. Exception from LDC 25-8-394(B) (Uplands Zone)

"This subsection applies in the extraterritorial jurisdiction and in the portions of the Lake, Rattan, and Brushy Creek watersheds that are in the zoning jurisdiction.

(1) Impervious cover for a single-family residential use with a minimum lot size of 5,750 square feet may not exceed:

(a) 45 percent; or

(b) if development intensity is transferred under Section 25-8-395 (*Transfer Of Development Intensity*), 50 percent.

(2) Impervious cover for a duplex or single-family residential use with a lot smaller than 5,750 square feet in size may not exceed:

(a) 55 percent; or

(b) if development intensity is transferred under Section 25-8-395 (*Transfer Of Development Intensity*), 60 percent.

(3) Impervious cover for a multifamily residential use may not exceed:

(a) 60 percent; or

(b) if development intensity is transferred under Section 25-8-395 (*Transfer Of Development Intensity*), 65 percent.

(4) Impervious cover for a commercial use may not exceed:

(a) 65 percent; or

(b) if development intensity is transferred under Section 25-8-395 (*Transfer Of Development Intensity*), 70 percent."

The applicant is requesting a modification to allow an overall impervious cover up to 62% net site area in the Uplands Zone.

8. Exception from LDC 25-4-154 (Street and Drainage Construction)

The applicant is requesting a modification such that street, alleys and pedestrian paths may be designed and constructed in accordance with the Traditional Neighborhood Criteria Manual, the Standards Specifications Criteria Manual or attached Exhibit G, Street Standards and Parking document.

9. Exception from LDC 25-6-171 (Standards for Design and Construction)

The applicant is requesting to delete this requirement.

10. Exception from LDC 25-6-172 (Arterial Streets)

The applicant is requesting to modify this requirement per Exhibit G of the proposed ordinance.

11. Exception from LDC 25-6-173 (Collector Streets)

The applicant is requesting to modify this requirement per Exhibit G of the proposed ordinance.

12. Exception from LDC 25-6-292(A) (Design and Construction Standards)

The applicant is requesting to modify this requirement to add the following language, "...except as modified by zoning."

13. Exception from LDC 25-6-321 (Existing Driveway)

The applicant is requesting to modify this requirement by adding the language, "...except as modified by zoning."

14. Exception from LDC 25-6 Article 7 (Off Street Parking and Loading)

The applicant is requesting to delete this section and replace it with Exhibit G of the proposed ordinance.

15. Exception from LDC 25-6 Article 7 (Off Street Parking and Loading)

The applicant is requesting to delete this section and replace it with Exhibit G of the proposed ordinance.

16. Exception from LDC 25-6 Appendix A (Tables of Off-Street Parking and Loading Requirements)

The applicant is requesting to delete this section and replace it with Exhibit G of the proposed ordinance.

- Protect critical environmental features. *This will protect environmental resources that are of critical importance.*

Conditions of protection are:

- Providing an IPM plan which addresses nuisance vegetation within CEF buffer zones
- Ensuring that no residential lots may include a CEF or be located within 50 feet of a CEF
- Ensuring that no residential lots will be located within a CEF buffer zone
- Ensuring that no disturbance of native vegetation occurs within the buffer zone
- Ensuring that no construction or placement of structures including buildings, sheds, pools, landscaping or gardens is allowed within a CEF buffer zone.
- Ensuring that no stormwater disposal or irrigation is allowed within the CEF buffer zone.
- Requiring erosion and sedimentation controls to be installed at the perimeter of all CEF buffers prior to the initiation of construction
- Requiring CEF buffers adjacent to drainageways to be platted separately and not within drainage or utility easements.
- Ensuring that no utilities are allowed within CEF buffers
- Providing fencing at least six feet in height at the edge of all CEF buffer zones. Each buffer must have a fence with an access gate with a lockable latch. Fencing at the edge of CEF buffers must be installed prior to the initiation of construction.
- Ensuring that water quality BMPs do not drain to CEF buffer zones but instead drain to areas where overland sheet flow may be maintained. Requiring level spreaders or similar structures for any discharges near CEF buffer zones
- Providing an Operation and Maintenance plan for the long term management of all CEF buffers which address items including trash removal, pet waste pickup, and inspections. This plan should also specify a long term funding mechanism and the responsible management entities throughout the construction and post-construction phases.
- Entering into a restrictive covenant with the City allowing access to the CEF buffer zones by City staff.
- Ensuring that wastewater and stormwater utility lines constructed within 500 feet of wetlands will be provided flow retards or Controlled Low Strength Material (CLSM) as fill around manholes as a means of preventing interception of subsurface groundwater flow away from wetlands, if determined necessary by City staff.
- Use of native and/or naturalized plants for landscaping. *This will result in water conservation and the reduction of chemicals used for plant maintenance.*
- Provide street trees along roadways. *This will reduce the heat island effect associated with increased impervious cover and density.*
- Eliminate the impervious cover assumption established by LDC 25-8-64 and use the maximum allowable impervious cover for the Single Family Residential Mixed Use

ARMBRUST & BROWN, L.L.P.

ATTORNEYS AND COUNSELORS

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DAVID B. ARMBRUST
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darmbrust@abaustin.com

July 18, 2007

Mr. Dave Anderson, Chair
and Members of Environmental Board
City of Austin
One Texas Center, 12th Floor
505 Barton Springs Road
Austin, TX 78704

Re: Lakeline Station Planned Unit Development (PUD) C814-06-0218 (the
"Application")

Dear Mr. Anderson:

This firm represents and this letter is submitted on behalf of the Applicant in the above referenced Application. The Lakeline Station PUD is approximately 326.798 acres in size and is located near the northwest corner of Parmer Lane and RM 620. The property is zoned I-RR and is currently vacant.

The proposed Lakeline Station PUD will allow for the creation of a mixed-use project that embraces sound urban design planning, density, and interesting architectural components. The Lakeline Station PUD is a transit oriented development (TOD) located adjacent to Capital Metro's Northwest Park and Ride station.

The developer believes that in order to provide a successful TOD, it should be designed to include walkable neighborhoods incorporating components of both mass transportation and different land uses to create a unique sense of place and higher quality of life for the residents of the community. Development of the property surrounding TODs is intrinsically urban and higher density to ensure the TOD's success. We believe the City should encourage high density development around each of the designated TOD sites. Density and ridership are absolutely the keys to making commuter rail a success. Less vehicles on the road will reduce carbon emissions on a regional scale.

The Lakeline Station PUD is unique in that it is one, if not the only designated TOD that is located within a suburban watershed. This creates a situation in which there are conflicting policy directives for the area: increased impervious cover/density to support mass transit vs. environmental concerns due to the watershed requirements. Based on the suburban watershed regulations within the City's code, the proposed approximately 62 percent impervious cover for

ARMBRUST & BROWN, L.L.P.

Page 2

the PUD equates to the entire area being developed as a commercial project. We believe that this is consistent with what should occur within a TOD and its surrounding area, in order to reduce the environmental impacts of sprawl.

The Lakeline Station PUD will include the following environmental features which are intended to enhance the quality of the community:

1. Provide wet ponds, which provide a more efficient method of removing pollutants compared to what is required by the Watershed Ordinance.
2. Water quality volume approximately twenty percent (20%) greater than required by City Code (see attached).
3. Capture and isolate rooftop drainage from the commercial areas used for irrigation purposes for park areas.
4. Compliance with a two star rating of Austin Energy's Green Building Program.
5. Implementation of an Integrated Pest Management Program.
6. Inclusion of approximately 68.6 acres of parks open space, including detention areas.
7. Required Water Conservation Strategies:

Indoor:

- IN - 1: Require all plumbing fixtures to perform at current plumbing code volumes;
- IN - 2: Require the use of submeters to bill for water in multi-family properties;
- IN - 3: Prohibit inefficient fixtures;
- IN - 4: Establish efficiency requirement for cooling tower management;
- IN - 5: Establish water consumption limits for car wash facilities and equipment;
- IN - 6: Establish efficiency standard for commercial clothes washers;

Outdoor:

- OU - 1: Limit frequency, timing, and method of outdoor watering;
- OU - 2: Require new residential irrigation systems to meet design standards and permitting requirements;
- OU - 5: Require homebuilders to offer a "WaterWise" landscape option;
- OU - 6: Require analysis of automatic irrigation systems;
- OU - 7: Require water audits for high-volume residential customers.

8. Critical Environmental Feature Protection.
 - a. No residential lots will be located within a CEF buffer zone.
 - b. No disturbance of native vegetation within the buffer zone.
 - c. No construction or placement of structures including buildings, sheds, pools, landscaping or gardens is allowed within a CEF buffer zone.
 - d. No stormwater disposal or irrigation is allowed with the CEF buffer zone.
 - e. The IPM shall include the removal of nuisance vegetation from the CEF buffer zones.
 - f. A restrictive covenant will be entered into with the City allowing access to the CEF buffer zones by City staff.

Although not quantified, improved air quality is an additional environmental benefit. A successful TOD removes vehicular traffic from the roadway network and encourages transit, bicycle, and pedestrian traffic. The removal of vehicular traffic directly correlates to improved air quality through the reduction of carbon emissions and noxious gases.

The Lakeline Station PUD includes several minor environmental variances, which are listed as follows:

1. Section 25-8-322, Clearing for a Roadway - Modify to allow clearing for widths up to twice the Right-of-way width.
2. Section 25-8-341, Cut Requirements - Modify to allow cuts in accordance with the attached Environmental Feature Exhibit.
3. Section 25-8-342, Fill Requirements - Modify to allow fills in accordance with the attached Environmental Feature Exhibit.
4. Section 25-8-394(B), Uplands Zone - Modify impervious cover limitations in the upland zone of the Brushy and Lake Creek Watersheds to match proposed site development standards and attached Environmental Feature Exhibit.

ARMBRUST & BROWN, L.L.P.

Page 4

Based on the benefits provided through this project, including the environmental standards noted above, the Lakeline Station PUD is superior to what could be developed under existing zoning.

Very truly yours,



David B. Armbrust

Enclosure

cc: Jerry Rusthoven
Patricia Foran
Wendy Walsh
Steve Levenson
Brian Avila
Art Coltrain
Peter Calthorpe
Romi Roy
Syd Xinos
David Miller
Kris Kasper
Lynn Ann Carley

WATER QUALITY ENHANCEMENTS

Increased Capture Volume

The developer has proposed to provide additional stormwater capture volume over and above that currently required by Code. The developer is committed to a minimum capture volume equal to the runoff from the one-year, three-hour event for the purposes of water quality and streambank erosion control. The following is a table comparing the current City of Austin Code requirements with that required by the Lakeline Station PUD:

Impervious Cover	COA Capture Volume (inches)	Required Capture Volume (inches) ¹	Increase
40%	0.70	0.75	7%
50%	0.80	0.92	15%
60%	0.90	1.08	20%
65%	0.95	1.16	22%
70%	1.00	1.24	24%
80%	1.10	1.41	28%
90%	1.20	1.57	31%
100%	1.30	1.74	34%

¹ Source: LCRA Nonpoint Source Pollution Control Technical Manual, July 2007

Lakeline Station has proposed an overall impervious cover of approximately 64% and based on the information provided in Exhibit H, the following table presents the treated volume for each development district in comparison to the volume currently required by City of Austin Code. The volumes associated with each development district are shown for comparison purposes as the various ponds on the site will handle portions of several districts. Each volume is presented with 100% of the district draining to the control with the exception of the Parks District. Actual volumes will be calculated as described in ECM, Section 1.6.2

Development District	COA Water Quality Volume (cubic feet)	PUD Water Quality Volume (cubic feet)	Increase
TOD Mixed Use Zone	92,786	118,423	27.6%
Attached / Cluster Residential	331,426	423,399	27.8%
Single Family Residential	549,875	677,929	23.3%
Parks / Open Space / Detention	53,807	53,807	0%
Civic	35,131	40,944	16.5%

Rainwater Harvesting

In an effort to provide additional controls for water quality, the developer has proposed the use of rainwater harvesting for 100% of the commercial use buildings within the TOD District. The system shall be designed to accept the water quality volume within seventy-two (72) hours after the end of the rainfall event and to detain and treat the water quality volume in accordance with the PUD requirements. The collected water may be used to irrigate landscaped or natural areas on the site. Irrigation systems shall be designed in accordance with standard irrigation practices considering such factors as soil type slope, and vegetation.



MEMORANDUM

To: Amber Mitchell
Watershed Protection and Development Review Department

From: Peter Marsh, P.E., Transportation Division
Public Works Department

Date: July 9, 2007

Subject: Lakeline Station PUD - Lakeline Boulevard paired couplet

The Transportation Division has given careful consideration to the proposal to construct the Lakeline Boulevard expansion through the Lakeline Station PUD as a "paired couplet", rather than duplicating lanes as mirror image of the existing to create a traditional divided arterial street.

While the Transportation Division will not object to the use of the Lakeline Boulevard paired couplet, it is important to point out that there appear to be some fundamental land use conflicts created by placing residential development between, and that fronts onto, the one-way pairs (that create the couplet) of a major arterial street.

The expansion of Lakeline Boulevard through the Lakeline Station PUD, if constructed as a "paired couplet", should comply with the following conditions:

- The Lakeline Boulevard expansion (the future eastbound roadway of the paired couplet) is designed to arterial standards, with a 45 mph design speed. This includes such design criteria as transition lengths at each end of the couplet, visibility sight distances, 'k' values, etc.
- The width of the future eastbound roadway of the paired couplet meets to the current design standards and corresponds with the existing Lakeline Boulevard cross-section, i.e. 30'-0" face-of-curb to face-of-curb, comprising 2 x 12'-0" travel lanes with a 6'-0" wide bike lane.
- Any on-street parking along Lakeline Boulevard, both the existing section and the proposed eastbound lanes of the "paired couplet", must comply with City Council resolution 20070125-016, which requires a 10'-0" wide maneuver lane separating the parallel on-street parking from the adjacent travel lane. The maneuver lane is exclusive of the width of the bike lane.
- The City of Austin reserves the right to remove on-street parking if there are accident problems or the safety of road users is compromised.

The following recommendations are suggested:

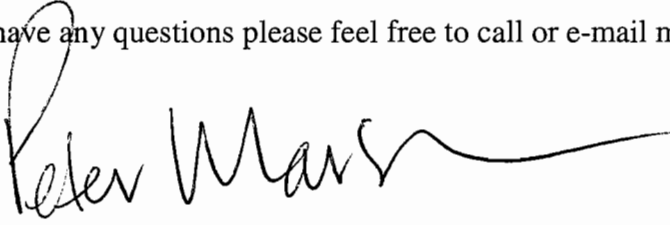
- At each intersection the travel lane widths plus the bike lane are maintained. The on-street parking and the associated maneuver lanes are protected by "bulb-outs".

EXHIBIT B-1

- Mid-block “bulb-outs”, with landscape trees, are provided to create parking bays rather than continuous parallel parking lanes.
- The parking bays and the maneuver lanes are constructed with a material that provides a color and/or texture contrast from the travel lanes to reduce the visual width of the roadway, ie with the travel lanes constructed with asphalt, the adjacent parking and maneuver lanes should be concrete (plain, colored or textured) or pavers etc.

Finally, the City follows the Texas MUTCD requirements for the installation of traffic signals, all-way stop controls, and pedestrian crosswalks. Traffic signals and all-way stop controls must meet the required warrants for installation. Pedestrian crosswalks, particularly when proposed on an arterial roadway such as Lakeline Boulevard, and not at signals and stop signs, will only be considered after completion of an engineering study.

If you have any questions please feel free to call or e-mail me.

A handwritten signature in black ink, reading "Peter Marsh". The signature is fluid and cursive, with a long horizontal flourish extending to the right.

Peter Marsh, P.E., Transportation Division
Public Works Department

xc. Tina Bui, Neighborhood Planning and Zoning Department



MEMORANDUM

TO: Wendy Rhoades, Case Manager
Members of the Planning Commission

DATE: August 9, 2007

SUBJECT: Variance Requests for Lakeline Station Area PUD
Case Number – C814-06-0218

Recommendation: Recommended with Conditions

The Lakeline Station Area PUD is a 317-acre development located in North Austin just south of Avery Ranch Boulevard, west of Parmer Lane and will include the completion of Lakeline Boulevard.

The proposed mixed-use development consists of the following land uses:

- ✧ 702 acres of TOD Mixed Use
- ✧ 1092 units Attached/Cluster Residential/Mixed Use
- ✧ 931 units of Single Family Residential/Mixed Use
- ✧ 12 acres of Civic Use
- ✧ 51 acres of Parks, Open Space and Detention

The Lakeline Station Area site is in the area covered by Senate Bill 1396 which states that “A municipality may not deny, limit, delay, or condition the use or development of land, any part of which is within an affected area, because of: (1) traffic or traffic operations that would result from the proposed use or development of the land; or (2) the effect that the proposed use or development of the land would have on traffic or traffic operations.”

The applicant for the above referenced PUD has requested the following variances from the transportation requirements of the Land Development Code. These variances are located in sections LDC 25-4 Subdivision and LDC 25-6 Transportation of Exhibit F – Code Modifications.

TR1.

Code Requirement: Section 25-4-171 requires each lot in a subdivision to abut a dedicated street.

Applicant's Request: The applicant proposes to modify this requirement for access to lots so that 1) each lot in a subdivision, except on a lot that fronts on a plaza or a green court and abuts an alley, shall abut a dedicated public street and so that 2) direct vehicular access from a single family residential lot to an alley is permitted and preferred. Lots containing multifamily, condominium, mixed use, commercial and civic uses do not require vehicular access from a lot to an alley.

Staff's Recommendation ~ Recommended with Conditions: Staff supports the recommended amendment with the following language added to the modification: the design of any and all lots where sole vehicular access is to an alley must be approved by the Director prior to preliminary plan or site plan approval.

Exhibit B-2

TR2.

Code Requirement: Section 25-4-154 provides for streets, alleys, and pedestrian paths to be designed and constructed in accordance with the Transportation Criteria Manual.

Applicant's Request: The applicant proposes to modify requirement so that streets, alleys, and pedestrian paths may be designed and constructed in accordance with the Traditional Neighborhood Criteria Manual, the Standards Criteria Manual, the Standard Specifications Manual or attached Exhibit G

Staff's Recommendation ~ Recommended with Conditions: Staff supports the use of Traditional Neighborhood Criteria as well as the proposed street standards in Exhibit G as these street and streetscape designs complement the intent of transit oriented development. Staff's support of the applicant's proposed Lakeline Boulevard design is conditional upon the criteria outlined in the memo dated July 9, 2007 from Peter Marsh, P.E. of the Public Works Department (see Exhibit B-1).

TR3.

Code Requirement: Section 25-6-171 describes the standards for the design and construction of streets.

Applicant's Request: The applicant proposes the deletion of this requirement and its replacement with the Traditional Neighborhood Criteria Manual, the Standards Criteria Manual, the Standard Specifications Manual, or attached Exhibit G.

Staff's Recommendation ~ Recommended with Conditions: Staff supports the recommended amendment with the conditions as stated above for the waiver to Section 25-4-154.

TR4.

Code Requirement: Section 25-6-172 requires arterial streets to comply with the Transportation Plan.

Applicant's Request: The applicant requests that the attached Exhibit G, Street Standards and Parking shall govern.

Staff's Recommendation ~ Recommended with Conditions: Staff supports the recommended amendment with the conditions as stated above for the waiver to Section 25-4-154.

TR5.

Code Requirement: Section 25-6-173 describes the requirements for the designation of collector streets.

Applicant's Request: The applicant requests that the attached Exhibit G, Street Standards and Parking shall govern.

Staff's Recommendation ~ Recommended: The Traditional Neighborhood Criteria and the street and streetscape design criteria proposed in Exhibit G support the intent of the proposed transit oriented development.

TR6.

Code Requirement: Section 25-6-292(A) sets the standards for the design, construction, alteration, or repair of sidewalks, driveway approaches, pavement, appurtenances on public property, or other facilities to provide access to adjoining property.

Applicant's Request: The applicant requests that the language "except as modified by zoning" be added to the Section to acknowledge that other changes made to 25-6 and the TCM affect this requirement.

Staff's Recommendation ~ Recommended: Staff has no objection to this additional language.

TR7.

Code Requirement: Section 25-6-321 specifies that the Council or Director may require an existing driveway to conform to Article 5 of Section 25-6 and the Transportation Criteria Manual as a condition of approval for an application for zoning, rezoning, or site plan approval.

Applicant's Request: The applicant requests that the language "except as modified by zoning" be added to the Section to acknowledge that other changes made to 25-6 and the TCM affect this requirement.

Staff's Recommendation ~ Recommended: Staff has no objection to this additional language.

TR8.

Code Requirement: Section 25-6 Article 7 (Off-Street Parking & Loading) provides the general regulations and design and construction standards for off-street parking and loading.

Applicant's Request: The applicant requests that the entire section is deleted and replaced with Exhibit G, Street Standards and Parking.

Staff's Recommendation ~ Recommended with Conditions: Staff supports the request on the condition that, unless expressly stated otherwise in Exhibit G, the regulations and design and construction standards for off-street parking and loading in Section 25-3 (Traditional Neighborhood District) shall apply.

TR9.

Code Requirement: Section 25-6 Appendix A provides the minimum parking and loading requirements for motor vehicles and bicycles.

Applicant's Request: The applicant requests that the entire section is deleted and replaced with Exhibit G, Street Standards and Parking.

Staff's Recommendation ~ Recommended with Conditions: Staff supports the request on the condition that, unless expressly stated otherwise in Exhibit G, the minimum parking and loading requirements for motor vehicles and bicycles in Section 25-3 (Traditional Neighborhood District) shall apply.

If you have any further questions or require additional information, please contact me at 974-3428.

A handwritten signature in black ink, appearing to read "Amber E. Mitchell". The signature is fluid and cursive, with the first name "Amber" being more prominent than the last name "Mitchell".

Amber E. Mitchell

Sr. Planner ~ Transportation Review
Watershed Protection & Development Review Department

LAKELINE STATION PUD EXHIBIT C: LAND USE PLAN

Austin, Texas
September 10, 2007



LEGEND:

- TOD Mixed Use
- Attached/Cluster Residential/Mixed Use
- Single Family Residential/Mixed Use
- Civic
- Parks
- Open Space & Detention
- Connector Streets

LAND USE SUMMARY

Development Districts	Net Acres	Lot Density	Max. Number of Units	Max. Impervious Cover (%)
1: TOD Mixed Use	23.4	15.8	43.0	79.1
2: Attached/Cluster Residential/MU	77.5	58.8	1211	84.8
3: Single Family Residential MU	140.7	98.1	853	73.0
4: Civic	1.1	1.1	1	10.0
5: Parks (Open Space, Detention)	63.5	0.5	0	5.8
Total (NET) Sub-Average	316.9		2,710	62.2%

SLOPE SUMMARY

Slope	Upland Area (Acres)
0-15 %	316.86
15-25 %	0.10
25-100 %	0.00

NOTES:

- See Exhibit D of the Ordinance for Site Development Standards.
- See Exhibits F, D, H, and the Ordinance for Code Modifications.
- An individual lot may have a maximum allowable impervious cover that exceeds allowed maximum for the property or development district so long as there is compliance with all said ordinances.

PACIFIC SUMMIT PARTNERS
Austin, Texas

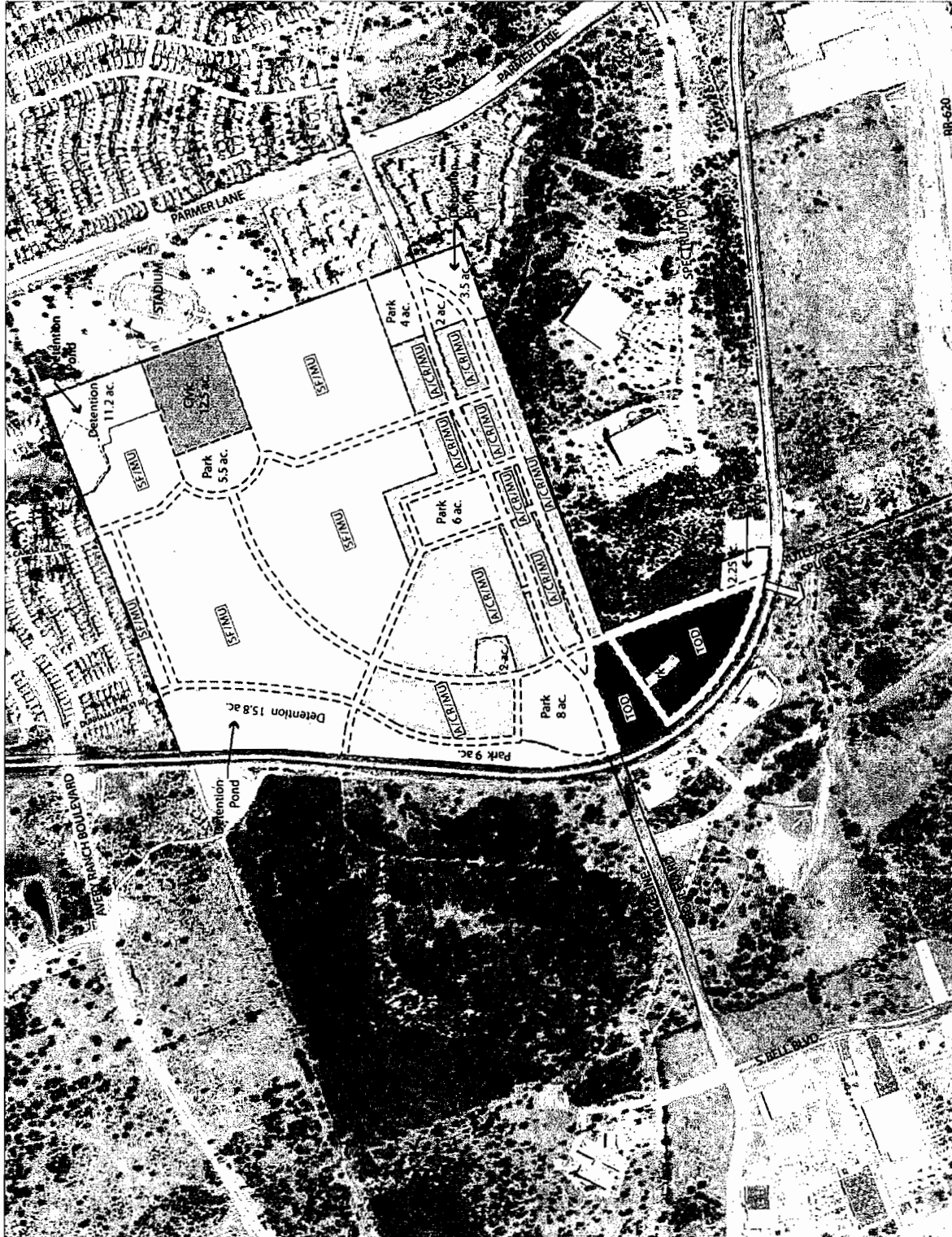


EXHIBIT
C

LAKELINE STATION PUD EXHIBIT D - SITE DEVELOPMENT REGULATIONS TABLE

Development District 1: TOD Mixed Use	Development District 2: Attached Cluster Residential/ MU
Maximum Height = 90 feet Maximum Floor to Area Ratio: 6 to 1 *Maximum District Impervious Cover = 79.1% Maximum Dwelling Units = 711	Maximum Height = 55 feet Maximum Floor to Area Ratio: 3 to 1 *Maximum District Impervious Cover = 84.8% Maximum Dwelling Units = 1211

Development District 3: Single Family Residential/ MU
Maximum Height = 45 feet Maximum Floor to Area Ratio: 2 to 1 *Maximum District Impervious Cover = 73.0% Maximum Dwelling Units = 853

BUILDING TYPE	Commercial/ Mixed use	Embedded Garage	Podium	Tuck-Under	Lakeline Townhome	Townhome Court	Live-Work Townhomes	*Green Court/ Paseo Court	Lakeline Small Lot	Lakeline Standard Lot	Estate Lot
GENERAL USE TYPES - See Exhibit E for Code Defined Zoning Uses	Commercial / Mixed use	Multifamily / Mixed use	Multifamily / Mixed use	Multifamily / Mixed use	Attached Single Family	Attached Single Family	Attached Single Family / Mixed Use	Detached Single Family	Detached Single Family	Detached Single Family	Detached Single Family
Minimum Lot Size	30,000sf	68,000 sf	30,000 sf	3000 sf	1,200 sf	1,200 sf	960 sf	1,800 sf	3,600 sf	4,500 sf	7,000 sf
Maximum Lot Size								7,500 sf	10,000 sf	10,000 sf	15,000 sf
Minimum Lot Depth	200 ft	340 ft	150 ft	100 ft	60 ft	60 ft	60 ft	60 ft	90 ft	90 ft	100 ft
Maximum Lot Width	150 ft	200 ft	200 ft	30 ft	20 ft	20 ft	16 ft	30 ft	40 ft	50 ft	70 ft
Maximum Front Setback	0 ft Commercial 10 ft Residential	0 ft Commercial 10 ft Residential	0 ft Commercial 10 ft Residential	0 ft Commercial 10 ft Residential	10 ft	10 ft	0 ft Commercial 10 ft Residential	10 ft	12 ft	12 ft	15 ft
Minimum Front Setback	0 ft Commercial 4 ft Residential	0 ft Commercial 4 ft Residential	0 ft Commercial 4 ft Residential	0 ft Commercial 4 ft Residential	4 ft	4 ft	0 ft Commercial 4 ft Residential	6 ft	8 ft	8 ft	10 ft
Minimum Interior Side Setback	0 ft	0 ft	0 ft	0 ft	0 ft	0 ft	0 ft	3 ft	3 ft	5 ft	5 ft
Minimum Corner Side Setback	0 ft	10 ft	10 ft	10 ft	N/A	N/A	N/A	N/A	7 ft	10 ft	10 ft
Minimum Rear Setback to Primary Building	20 ft	20 ft	20 ft	4 ft	4 ft	4 ft	4 ft	4 ft	10 ft	15 ft	20 ft
Minimum Frontage Percentage	75%	75%	75%	75%	75%	75%	75%	no requirement	no requirement	no requirement	no requirement
Minimum Porch/Stoop Setback	0 ft	0 ft	0 ft	4 ft	4 ft	4 ft	0 ft Commercial 4 ft Residential	4 ft	5 ft	5 ft	5 ft

*An individual Lot may have a Maximum Allowable Impervious Cover that exceeds that allowed for the Property or Development District so long as Owner complies with maximum impervious cover for each of the Development Districts.

LAKELINE STATION PUD

EXHIBIT D – SITE DEVELOPMENT REGULATIONS NOTES

1. In TOD Mixed Use Development District, all Commercial and/or Mixed-use buildings with ground floor commercial use shall follow the site development regulations specified on page D-1 of Exhibit D. If a Mixed-use or Commercial building has retail/commercial uses on the ground floor, the minimum setback from the street will be zero feet.
2. Non-Residential Uses are allowed in within the Attached/ Cluster residential/ Mixed Use and Single Family/ Mixed Use Districts if located within a the Live-Work Building type (as shown in Figure 1 below). In addition, the live-work building type within these two districts shall be allowed only on Lakeline Boulevard and on Connector Streets, as shown in Exhibit C; or on corner lots at intersections with a connector street.

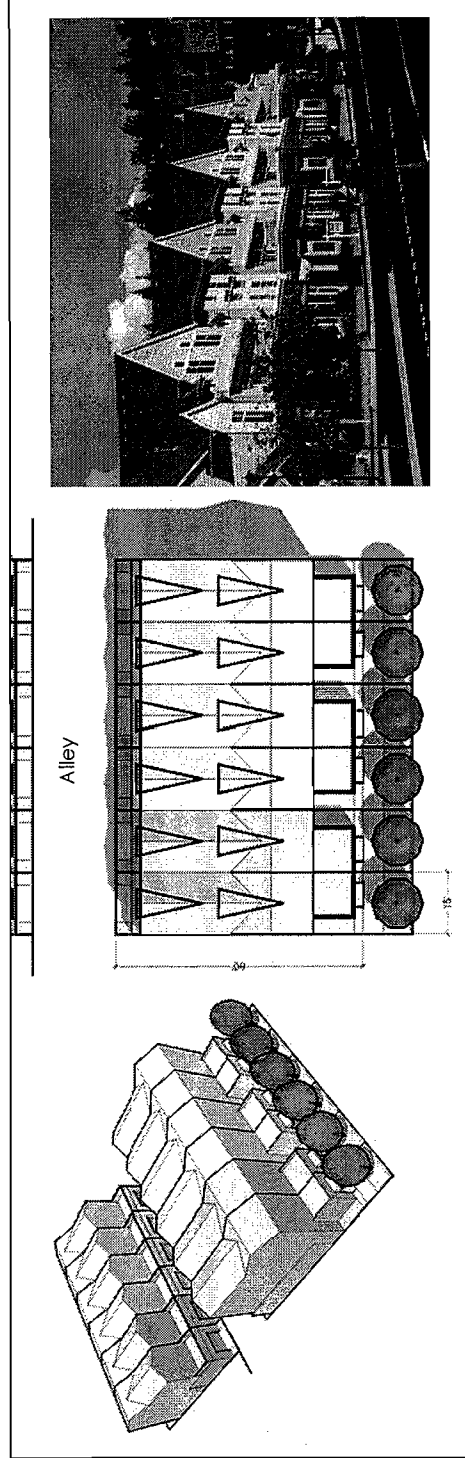


Figure 1: Example of Live-work Building Type. Live-work homes offer a compact ownership residential-office opportunity without the noise impacts that can occur in condominiums with neighbors above or below. Each unit will have an individual ground-floor entry addressing the street, a work-space/ commercial space in front looking on to the street with minimal or no setback, a private garage, and private parcel of land. Garages are alley-accessed.

3. Lots within the Single Family Residential/ Mixed Use Development District which are 40 feet wide or less must be 'rear-loaded' and only lots wider than 40 feet may be 'front-loaded'.
4. No drive-through services shall be permitted within this development.
5. For all mixed-use, commercial or other non-residential use buildings, Chapter 25-2, Subchapter E, Article 3: Building Design Standards apply. If the applicant seeks a minor modification or alternative equivalent compliance from one or more of the requirements of Article 3, that approval shall be in accordance with Chapter 25-2, Subchapter E, Article 1, Sections 1.4 and 1.5.

6. Garage Placement Criteria:

- A) Garage side yard setbacks may be reduced to zero feet provided that applicable fire rating and building codes are met. The wall of a garage built adjacent to a zero lot line or within 3 feet of a common side lot line must be solid and opaque, and may not contain an opening.
- B) Residential garages shall be positioned to de-emphasize their visual presence on the street. This will allow the active, visually interesting features of the house, to dominate the streetscape. Garages may be sited in the following ways (as shown in Figure 2):
 - Rear-Alley loaded garages.
 - Front-Loaded Side Drive garages (Minimum 35' behind front wall plane of the residence) with maximum 10' wide curb cut at the street.
 - Front-Loaded Recessed garages (Minimum 5' behind front wall plane of the residence) with maximum 18' curb cut at the street.

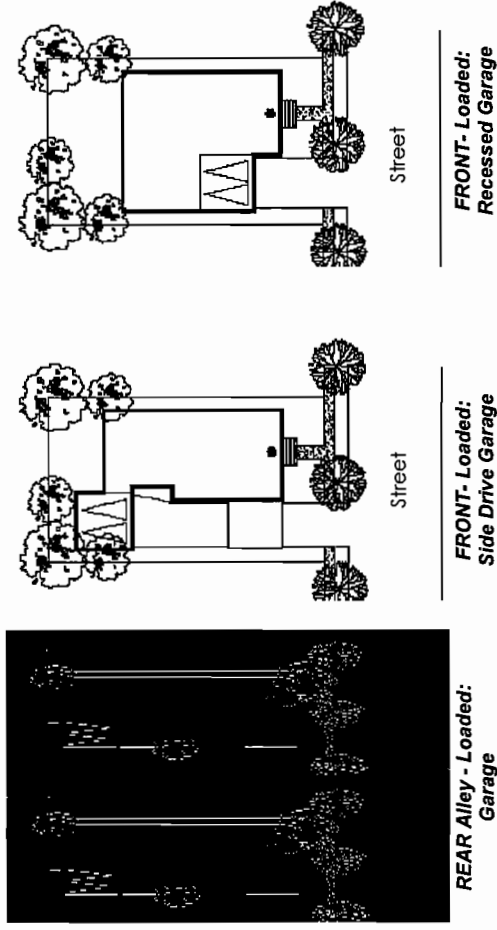


FIGURE 2: Examples of Garage Placement

7. Guidelines For Development within the TOD Mixed Use Development District:

A) Building Entrances, Minimum Ground Floor Height, Fenestration

Building entrances are required on connector streets; and on streets with transit service (train or bus).

- This paragraph applies to a building that is constructed along a front yard or street side yard setback line. For a depth of at least 20 feet, the minimum distance between the finished ground floor of the building and the structural portion of the ceiling is 15 feet. This requirement does not apply if the building is subject to Article 10 (Compatibility Standards) or if the director determines that the requirement is impractical because of site constraints.
- This paragraph applies to a commercial or mixed-use building. For a ground level wall that faces a public street, at least 50 percent of the wall area that is between two and ten feet above grade must be constructed of glass with a visible transmittance rating of 0.6 or higher.

B) Sidewalks in the TOD Mixed Use Development District

In order to create an environment that is supportive of pedestrian and transit mobility, public sidewalks shall be located along both sides of all streets in the TOD Mixed Use Development District. No sidewalk shall be less than 15 feet in width, unless otherwise approved as part of the site plan review process. The 15 foot minimum requirement shall apply regardless of the available right-of-way. Where required, the sidewalk shall extend onto private property to fulfill the 15-foot minimum requirement, with a sidewalk easement provided. Sidewalks shall consist of two zones: a street tree/furniture zone located adjacent to the curb and a clear zone, as shown in Figures 3, 4 and 5. The following standards shall apply to these zones:

(1) Street Tree/Furniture Zone

- a. The street tree/furniture zone shall have a minimum width of eight feet (from face of curb) and shall be continuous and located adjacent to the curb.
- b. The zone shall be planted with street trees at an average spacing not greater than 30 feet on center, or up to 60 feet on center if parallel or head-in parking is provided pursuant to Subchapter E: Design Standards and Mixed Use, Section 2.2.2.B.5. Trees shall be selected in conformance with Appendix N: City of Austin Preferred Plant List of the Environmental Criteria Manual.
- c. In addition, the zone is intended for the placement of street furniture including seating, street lights, waste receptacles, fire hydrants, traffic signs, newspaper vending boxes, bus shelters, bicycle racks, public utility equipment such as electric transformers and water meters, and similar elements in a manner that does not obstruct pedestrian access or motorist visibility.

(2) Clear Zone

The clear zone shall be a minimum width of 7-feet, shall be hardscaped, shall be located adjacent to the street tree/furniture zone, and shall comply with ADA and Texas Accessibility Standards. The clear zone shall be unobstructed by any permanent or nonpermanent element for a minimum width of seven feet and a minimum height of eight feet, as shown in Figures 3, 4 and 5.

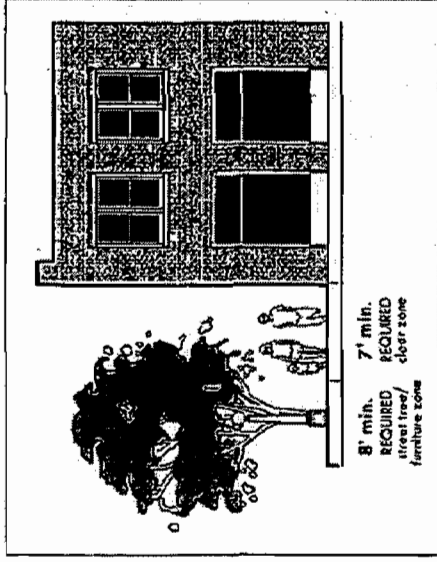


Figure 3. Sidewalk Requirements in TOD Mixed Use District. Street trees are required in this District with an average spacing not greater than 30 ft on center.



Figure 4. Street tree/furniture zone example



Figure 5. Clear zone example

C) Supplemental Zone (Optional)

A supplemental zone may be provided at the option of the applicant between the street-facing facade line and the required clear zone, as shown in Figures 6, 7 and 8. The following standards apply to supplemental zones:

- 1) If a supplemental zone is provided, up to 30 percent of the linear frontage of the supplemental zone may be a maximum of 30 feet wide, and the remainder of the supplemental zone shall be a maximum of 20 feet wide.
- 2) The following elements may be located within the supplemental zone:
 - a. Accessory outdoor dining, provided that the dining area may be separated from the sidewalk only with planters, shrubs, or fencing with a maximum height of 42 inches, as shown in Figure 8);
 - b. Balconies, pedestrian walkways, porches, handicap ramps, and stoops; provided, however, that no such feature shall extend beyond the supplemental zone without a license agreement;
 - c. Terraces, provided that they have a maximum finished floor height of 24 inches above the sidewalk elevation and shall be surrounded by a guardrail that meets city specifications;
 - d. Landscape and water features;
 - e. Plazas; and
 - f. Incidental display and sales.

3) Any features in the supplemental zone must not obstruct the open pedestrian connection between the building's primary entrance and the clear zone.

4) If a supplemental zone is provided, the building can be pushed back farther than the stipulated maximum front setback, but not farther than the edge of the supplemental zone (20'-30' max. from the setback line).

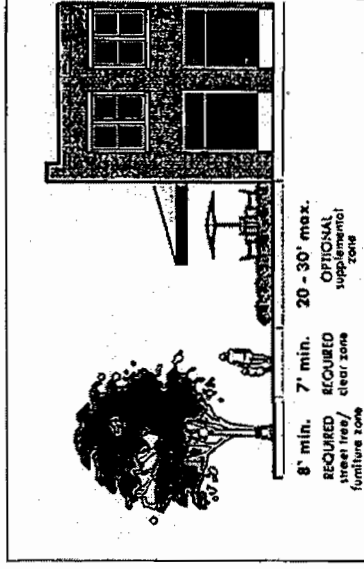


Figure 6: Optional Supplemental Zone (Section)

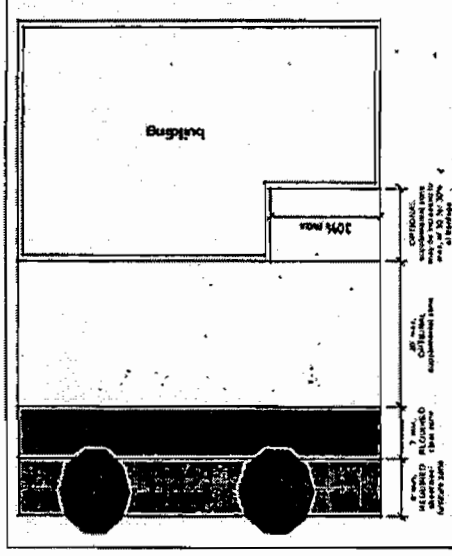


Figure 7: Optional Supplemental Zone may be expanded to 30 ft for a maximum of 30% of frontage.



Figure 8: Example of Supplemental Zone outdoor dining

D)

Building Placement

(Ref: Chapter 25-2, Subchapter E: Design Standards and Mixed Use, Article 2: Site Development Standards, Section 2.2, Subsection 2.2.2; and Article 5: Definitions)

- 1) General Building Placement Standard:

At least 75 percent of the "net frontage length" of the property along the street must consist of continuous building facade built up to the clear zone, or the supplemental zone if one is provided. (See adjoining Figure)

- 2) "Net frontage length" (defined in the City Code) is determined by subtracting required Internal Circulation Routes, side or compatibility setbacks, easements, drive aisles, sidewalks, and stairs that occur at the building perimeter from the total property length, as measured along the front lot line from property line to property line. See adjoining figure (far right). In the case of a curved corner, the Director may determine the end point for purposes of measuring net frontage.



Figure 9: Examples of permitted building placement in the TOD Mixed Use Development District. Parking is not permitted in the hatched area between the street-facing façade and the sidewalk.

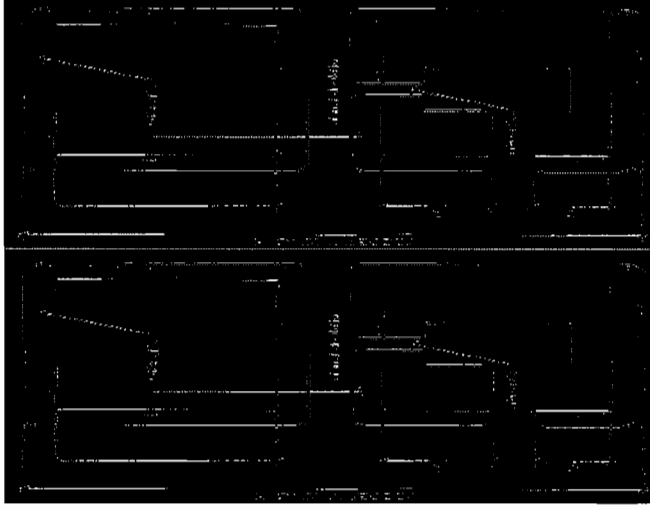


Figure 10: The net frontage length for the property is the total of lengths A, B and C. Required internal circulation routes, drive aisles and perimeter sidewalks are not included.

E) Utilities:

- 1) All utility lines shall be underground from the building to the property line. Utility lines within the right-of-way shall be placed underground or relocated to the rear of the site to the maximum extent practicable. Figure 11 illustrates this principle.
- 2) Where electric utilities remain overhead and are located behind the curb, an overhead utility zone shall be provided so that no portion of the building is located within a 10-foot radius of the energized conductor. This overhead utility zone shall be in addition to the minimum street tree/furniture zone, clear zone, and supplemental zone (if provided). Options for street tree planting and sidewalk placement in combination with overhead utilities are illustrated in the Figures 12 and 13.
- 3) On lots with a depth of 120 feet or less and where electric utilities remain overhead and are located behind the curb, alternative trees may be used so that the trees can be located beneath, rather than offset from, the overhead electric utilities. Alternate trees shall be selected in conformance with Appendix N: City of Austin Preferred Plant List from the Environmental Criteria Manual.

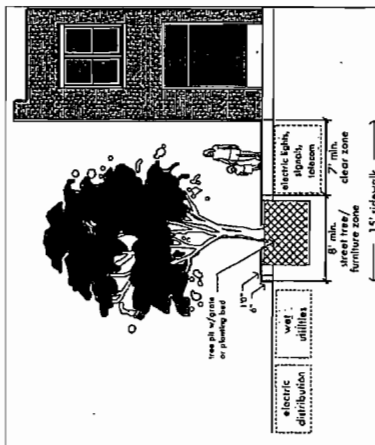


Figure 11: Underground Utilities

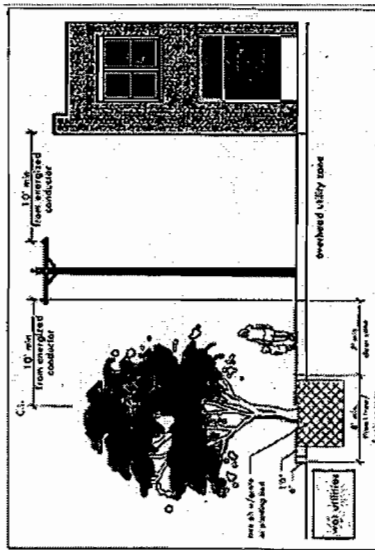


Figure 12: Overhead Utility Zone

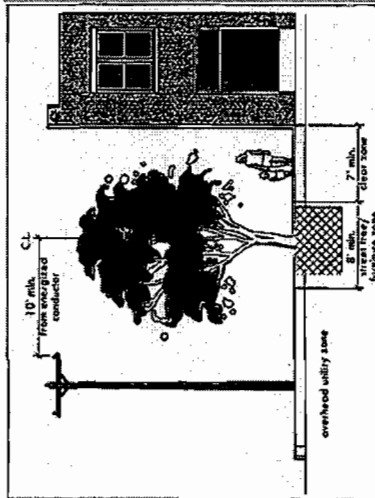


Figure 13: Overhead Utility Zone at Curb

LAKELINE STATION PUD
EXHIBIT E - ZONING USE SUMMARY TABLE

Land Use	TOD Mixed Use District	Attached/Cluster Residential/Mixed Use District	Single Family Residential/Mixed Use District	Parks/Open Space/ Detention	Civic
RESIDENTIAL USES					
Bed and Breakfast Residential (Group 1)	P	P	P	NP	NP
Bed and Breakfast Residential (Group 2)	P	P	P	NP	NP
Condominium Residential	P ^a	P	P	NP	NP
Duplex Residential	NP	NP	P	NP	NP
Group Residential	P ^a	P	P	NP	NP
Mobile Home Residential	NP	NP	NP	NP	NP
Multifamily Residential	P	P	P	NP	NP
Retirement Housing (Small Site)	P ^a	P	P	NP	NP
Retirement Housing (Large Site)	P ^a	P	P	NP	NP
Single-Family Attached Residential	NP	NP	P	NP	NP
Single-Family Residential	NP	P	P	NP	NP
Small Lot Single-Family Residential	NP	P	P	NP	NP
Townhouse Residential	NP	P	P	NP	NP
Two-Family Residential	NP	NP	P	NP	NP
COMMERCIAL USES					
Administrative and Business Offices	P	P	P	NP	NP
Agriculture Sales and Services	NP	NP	NP	NP	NP
Art Gallery	P	P	P	NP	P
Art Workshop	P	P	NP	NP	NP
Automotive Rentals	C ^b	NP	NP	NP	NP
Automotive Repair Services	C ^c	NP	NP	NP	NP
Automotive Sales	NP	NP	NP	NP	NP

EXHIBIT E

Land Use	TOD Mixed Use District	Attached/Cluster Residential/Mixed Use District	Single Family Residential/Mixed Use District	Parks/Open Space/ Detention	Civic
Automotive Washing (of any type)	NP	NP	NP	NP	NP
Bail Bond Services	NP	NP	NP	NP	NP
Building Maintenance Services	P	NP	NP	NP	NP
Business or Trade School	P	NP	NP	NP	NP
Business Support Services	P	NP	NP	NP	NP
Campground	NP	NP	NP	NP	NP
Carriage Stable	NP	NP	NP	NP	NP
Cocktail Lounge	P	NP	NP	NP	NP
Commercial Blood Plasma Center	P	NP	NP	NP	NP
Commercial Off-Street Parking	C ^d	NP	NP	NP	C
Communications Services	P	P	NP	NP	NP
Construction Sales and Services	NP	NP	NP	NP	NP
Consumer Convenience Services	P	C	P	NP	NP
Consumer Repair Services	P	P	P	NP	NP
Convenience Storage	C	NP	NP	NP	NP
Custom Manufacturing and Printing	P	P	NP	NP	NP
Drop-Off Recycling Collection Facility	NP	NP	NP	NP	NP
Electronic Prototype Assembly	P	C	NP	NP	NP
Equipment Repair Services	NP	C	NP	NP	NP
Equipment Sales	NP	C	NP	NP	NP
Exterminating Services	P	NP	NP	NP	NP
Financial Services	P	P	P	NP	NP
Food Preparation	P	NP	NP	NP	NP
Food Sales	P	NP	NP	NP	NP
Funeral Services	NP	NP	NP	NP	NP
General Retail Sales (Convenience)	P	C	NP	NP	NP
General Retail Sales (General)	P	C	NP	NP	NP
Hotel-Motel	P	NP	NP	NP	NP

Land Use	TOD Mixed Use District	Attached/Cluster Residential/Mixed Use District	Single Family Residential/Mixed Use District	Parks/Open Space/ Detention	Civic
Indoor Entertainment	P	NP	NP	NP	P
Indoor Sports and Recreation	P	NP	NP	NP	NP
Kennels	NP ^e	C	NP	NP	NP
Laundry Services	NP	NP	NP	NP	NP
Liquor Sales	P	NP	NP	NP	NP
Marina	NP	NP	NP	NP	NP
Medical Office - exceeding 5,000 sq. ft. of gross floor area	P	NP	NP	NP	NP
Medical Offices - not exceeding 5,000 sq. ft. of gross floor area	P	C	NP	NP	NP
Monument Retail Services	NP	NP	NP	NP	NP
Off-Site Accessory Parking	C	NP	NP	NP	C
Outdoor Entertainment	C	NP	NP	P	P
Outdoor Sports and Recreation	NP	NP	NP	P	P
Pawn Shop Services	NP	NP	NP	NP	NP
Personal Improvement Services	P	C	NP	NP	NP
Personal Services	P	C	NP	NP	NP
Pet Services	P	C	NP	NP	NP
Plant Nursery	P	NP	NP	NP	NP
Printing and Publishing	P	C	NP	NP	NP
Professional Office	P	P	P	NP	NP
Recreational Equipment Maintenance & Store	NP	NP	NP	NP	NP
Recreational Equipment Sales	NP	NP	NP	NP	NP
Research Assembly Services	P	NP	NP	NP	NP
Research Services	P	NP	NP	NP	NP
Research Testing Services	NP	NP	NP	NP	NP
Research Warehouse Services	NP	NP	NP	NP	NP
Restaurant (Limited)	P	C	NP	NP	NP
Restaurant (General)	P	C	NP	NP	NP
Service Station	P ^f	NP	NP	NP	NP
Software Development	P	P	NP	NP	NP

Land Use	TOD Mixed Use District	Attached/Cluster Residential/Mixed Use District	Single Family Residential/Mixed Use District	Parks/Open Space/ Detention	Civic
Special Use Historic	NP	NP	NP	NP	NP
Stables	NP	NP	NP	NP	NP
Theater	P	NP	NP	NP	NP
Vehicle Storage	NP	NP	NP	NP	NP
Veterinary Services	NP	C	NP	NP	NP
INDUSTRIAL USES					
Basic Industry	NP	NP	NP	NP	NP
Custom Manufacturing	P	NP	NP	NP	NP
General Warehousing and Distribution	NP	NP	NP	NP	NP
Light Manufacturing	NP	NP	NP	NP	NP
Limited Warehousing and Distribution	NP	NP	NP	NP	NP
Recycling Center	NP	NP	NP	NP	NP
Resources Extraction	NP	NP	NP	NP	NP
CIVIC USES					
Administrative Services	P	P	P	NP	P
Aviation Facilities	NP	NP	NP	NP	NP
Camp	NP	NP	NP	NP	NP
Cemetery	NP	NP	NP	NP	NP
Club or Lodge	P	NP	NP	NP	P
College and University Facilities	P	NP	NP	NP	P
Communication Service Facilities	P	P	P	NP	NP
Community Recreation (Private)	P	P	P	P	P
Community Recreation (Public)	P	P	P	P	P
Congregate Living	P	NP	NP	NP	P
Convalescent Living	P	NP	NP	NP	P
Convention Center	NP	NP	NP	NP	NP
Counseling Services	P	NP	NP	NP	NP
Cultural Services	P	P	NP	P	P
Day Care Services (Commercial)	P	NP	NP	NP	NP
Day Care Services (General)	P	C	C	NP	NP

Land Use	TOD Mixed Use District	Attached/Cluster Residential/Mixed Use District	Single Family Residential/Mixed Use District	Parks/Open Space/ Detention	Civic
Day Care Services (Limited)	P	C	P	NP	NP
Detention Center	NP	NP	NP	NP	NP
Employee Recreation	NP	NP	NP	NP	NP
Family Home	P	P	P	NP	NP
Group Home, Class I (General)	P	P	P	NP	NP
Group Home, Class I (Limited)	P	P	P	NP	NP
Group Home, Class II	P	P	P	NP	NP
Guidance Services	P	NP	NP	NP	NP
Hospital Services (General)	C	NP	NP	NP	NP
Hospital Services (Limited)	P	NP	NP	NP	NP
Local Utility Services	P	C	P	P	P
Maintenance and Service Facilities	NP	NP	NP	NP	NP
Major Public Facilities	C	C	C	NP	C
Major Utilities Facilities	C	C	C	NP	NP
Military Installations	NP	NP	NP	NP	NP
Park and Recreation Services (General)	P	P	P	P	P
Park and Recreation Services (Special)	P	P	P	P	P
Postal Facilities	P	P	NP	NP	P
Private Primary Educational Facilities	C	C	C	NP	P
Private Secondary Educational Facilities	C	C	C	NP	P
Public Primary Educational Facilities	P	C	P	NP	P
Public Secondary Educational Facilities	P	C	P	NP	P
Railroad Facilities	NP	NP	NP	NP	NP
Religious Assembly	P	P	P	P	P
Residential Treatment	C	C	C	NP	P

Land Use	TOD Mixed Use District	Attached/Cluster Residential/Mixed Use District	Single Family Residential/Mixed Use District	Parks/Open Space/ Detention	Civic
Safety Services	P	P	P	NP	P
Telecommunication Tower – subject to SS 25-2-839 (13-2-273)	PC ^g	NP	NP	C	C
Transitional Housing	C	NP	NP	NP	NP
Transportation Terminal	P	NP	NP	NP	NP
All other Civic Uses	NP	NP	NP	C	C
AGRICULTURAL USES					
Urban Farm	NP	NP	NP	NP	NP

P = Permitted NP = Not Permitted C = Conditional Use

- (a) 25-3-105 (D) An open colonnade may extend into the front setback a maximum of five feet (5'). An unenclosed balcony with a minimum clearance of nine feet (9') above finished grade may extend five feet (5') over a public sidewalk. An awning or walkway covering with a minimum clearance of eight feet (8') above finished grade may extend five feet (5') over a public sidewalk. 25-3-105 (G) A residential use with street level living space must have a finished first floor elevation not less than eighteen inches (18") above the elevation of the sidewalk at the front lot line. A residential use may not front at ground level on a square.
- (b) 25-3-105 (A) An automotive rental use may keep not more than 20 vehicles on site.
- (c) 25-3-105 (B) An automotive repair services use may not exceed 2,400 square feet of gross building area.
- (d) 25-3-105 (E) A commercial off-street parking use may not exceed one acre in site size. Not more than one commercial off-street parking use site may be located in a block. A site must be screened from the street by low hedges or walls not less than three feet (3') and not more than four feet (4') in height.
- (e) 25-3-105 (F) A kennel use must be conducted entirely within an enclosed structure.
- (f) 25-3-105 (H) A service station use may have the capability of fueling not more than eight vehicles at one time.
- (g) 25-3-105 (I) A telecommunications tower must be located on top of a building or be an architectural component of the building. Free-standing towers are prohibited.

LAKELINE STATION PUD
EXHIBIT F – STREET STANDARDS AND PARKING REGULATIONS

STREET STANDARDS

Key	Street Type	ROW	No. of Lanes	Min. Curb Basis	Sidewalk	Paving	Parking	Bike Lanes	Design Speed	Corresponding TND Street Type
A	Couplet ("Lakeline Boulevard")	2 nos. @ 74' each	2 one-way lanes each (4 total)	14'	6-foot	46'	2 sides	Yes	30 mph	-
B	Connector Street (except TOD MU District and at Open Space)	64-foot	2	14'	6-foot	36'	2 sides	No	25 mph	Neighborhood Center Main Street
C	Connector Street at TOD Mixed Use District	70-foot	2	15'	7-foot	40'	2 sides	No	25 mph	-
D	Connector Street at Open Space	52-foot	2	0'/14'	6-foot	38'	2 sides	No	25 mph	-
E	Local Street with Driveways	50-foot	1 (shared for two-way traffic)	10'	5-foot	30'	2 sides	No	25 mph	-
F	Local Street without Driveways	54-foot	2	10'	5-foot	34'	2 sides	No	25 mph	Mixed residential Avenue
G	Local Street at TOD Mixed Use District	66-foot	2	15'	7-foot	36'	2 sides	No	25 mph	-
H	Local Street at Open Space	37-foot	1 (shared for two-way traffic)	0'/10'	5-foot	27'	1 side	No	25 mph	-
I	Residential Alley	20-foot	1 (shared for two-way traffic)	0'	No	20'	No	No	N/A	Mixed Residential Alley

1. Construction of cul-de-sacs is discouraged. In case a temporary cul-de-sac is required for construction purposes, the design requirements contained in the City of Austin Transportation Criteria Manual Section 1.3.2 should be followed.
2. Street cross-sections may be modified with the approval of both Watershed Protection and Development Review and Neighborhood Planning and Zoning Departments.
3. Except as otherwise set forth above, the PUD street designs shall comply with the street standards set forth in the Traditional Neighborhood District Criteria Manual (i.e. design speed, typical ADT range, etc.)
4. On street parking shall be allowed along the Couplet, as well as Connector Streets and Local Streets.

LAKELINE STATION AREA PLAN PUD
EXHIBIT F – STREET STANDARDS AND PARKING REGULATIONS

PARKING REGULATIONS

- A. For the TOD Mixed Use Development District, parking regulations proposed for various uses are described as follows:
- 1) Off-street parking is prohibited between the Right-of-Way and the corresponding street-facing façade line. (Ref: Chapter 25-2, Subchapter E, Article 2, Subsection 2.2.2 E.1)
 - 2) All Use-types within this zone may apply adjacent on-street parking towards the minimum parking requirements within this district.
 - 3) A commercial building use must provide at least one bicycle parking space for every 5 motor-vehicle parking spaces.
 - 4) Except as approval by the Director, parking in alleys is prohibited.
 - 5) At grade structured parking or parking lots must be 'lined' with commercial or residential uses (with building entrances) or must be visually screened from the street by a landscape buffering in accord with Section 25-2-1006 of the LDC between the 'clear zone' (or the supplemental zone if provided) and the parking area. The buffering method chosen must include shade trees.
- B. For the Attached/ Cluster Residential/ Mixed Use and Single Family/ Mixed-Use Development Districts, parking requirements from Section 25-3-83(A)(1) and (3) of the City code shall apply.
- C. The Director shall determine the parking requirement for an amphitheater and/or auditoriums based on similar uses, location and characteristics of the use, and the appropriate traffic engineering and planning data.
- D. The Director shall determine the parking requirement for community centers auditoriums based on similar uses, location and characteristics of the use, and the appropriate traffic engineering and planning data.
- E. The Director shall determine the parking requirement for neighborhood parks, pavilions, picnic shelters or tables, playgrounds, swimming pools, boat rental facilities, basketball/sports courts, or bike rental facilities based on similar uses, location and characteristics of the use, and the appropriate traffic engineering and planning data.
- F. Unless expressly stated in this PUD, the general regulations, design, and construction standards for off-street parking and loading in Section 25-3 of the City Of Austin Code shall apply. However, the Director may determine the parking requirement for any use not listed in this subsection.

LAKELINE STATION
PUD
Exhibit G - Street Cross
Section Illustrations

Austin, Texas

April 06, 2007



Layout of
Lakeline Couplet

NOTE: Currently planned Lakeline
Boulevard R.O.W. and 30' Curb shown
as dashed lines.

PACIFIC SUBURBAN PARTNERS
Austin, Texas

G-1

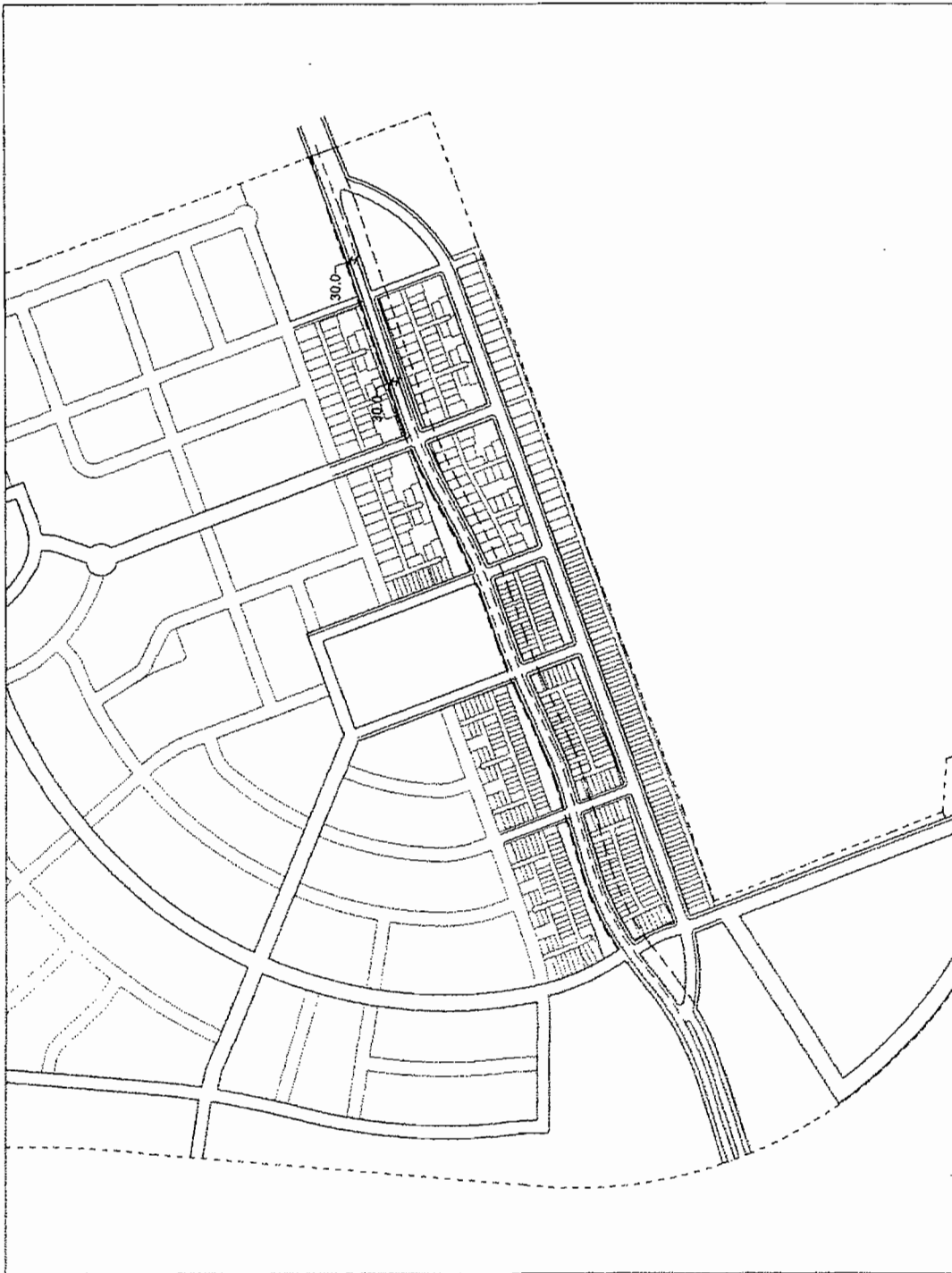


EXHIBIT G

LAKELINE STATION
PUD
Exhibit G - Street Cross
Section Illustrations

Austin, Texas

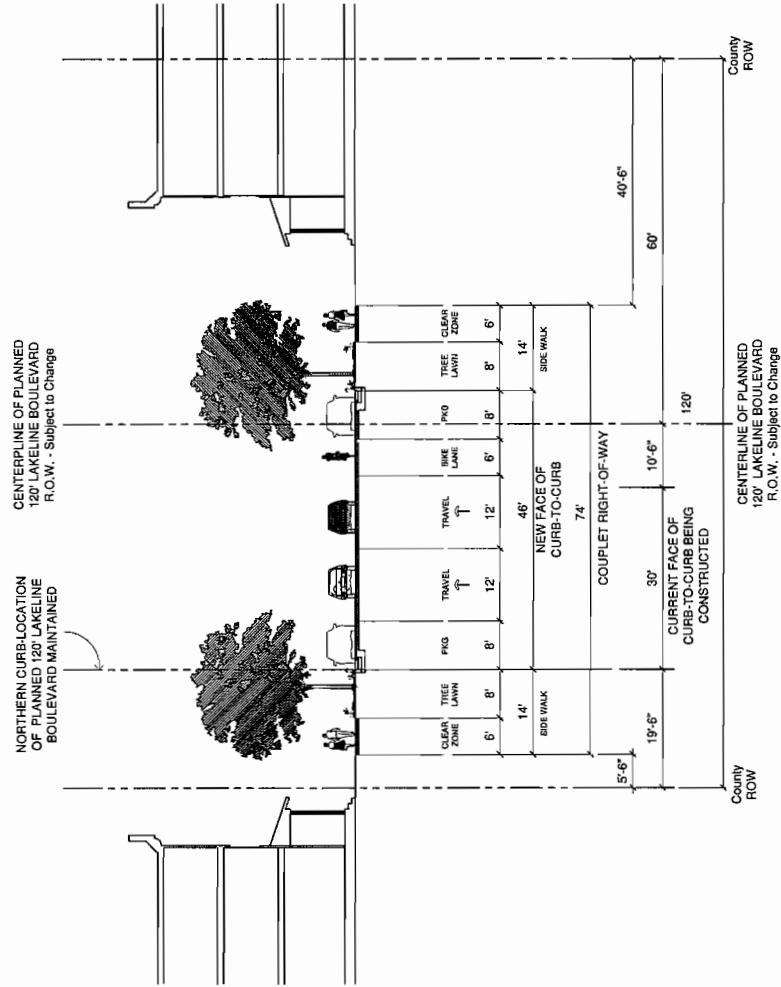
April 06, 2007



A. Couplet Section for "Lakeline Boulevard"

SUMMIT PROPERTIES
Irvine, California

G-2



2470 POST STREET GAITHER, PA 9105-1154
510-542-8690 TEL: 510-542-4444 FAX: 510-542-4444 CALTOPPE.COM

CALIHORPE ASSOCIATES
TELEPHONE PLANNING ASSOCIATES

LAKELINE STATION PUD Exhibit G - Street Cross Section Illustrations

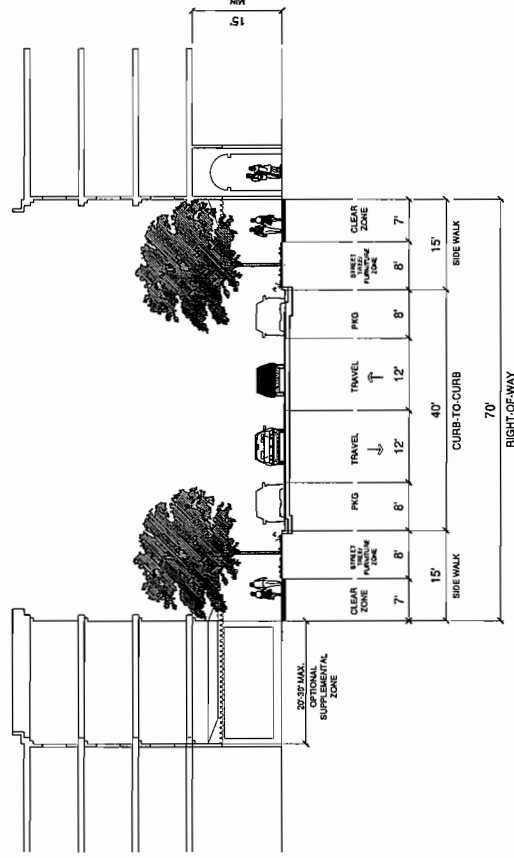
Austin, Texas

April 06, 2007



0 10' 20'

C. Connector Street at TOD Mixed Use Zone



CONNECTOR STREET @ TOD MIXED USE ZONE

SUMMIT PROPERTIES
Irvine, California

LAKELINE STATION PUD Exhibit G - Street Cross Section Illustrations

Austin, Texas

April 06, 2007

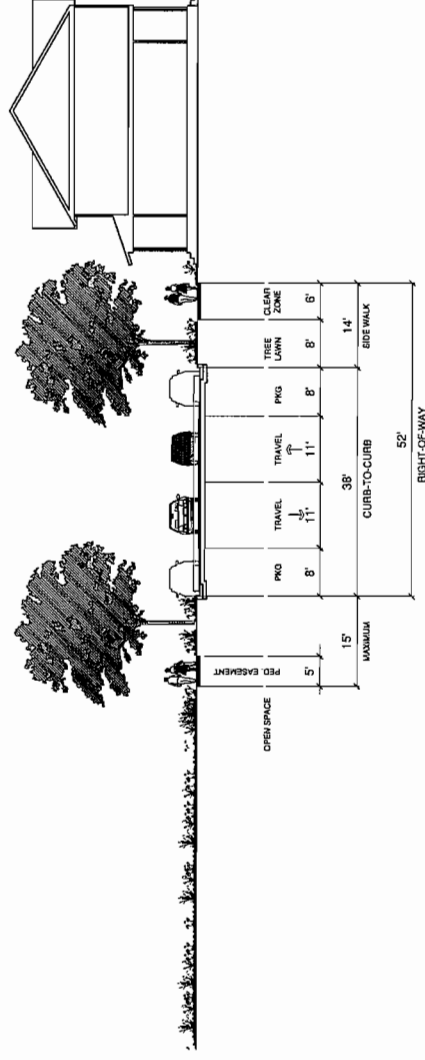


D. Connector Street at Open Space

NOTE: A minimum 5' wide perpetual public access easement shall be provided outside the street right-of-way on the open space side. The easement will be required at the time of subdivision and should not meander more than 15' from the right-of-way line and must intersect at street intersections.

SUMMIT PROPERTIES
Irvine, California

G-5



CONNECTOR STREET
(at Open Space)

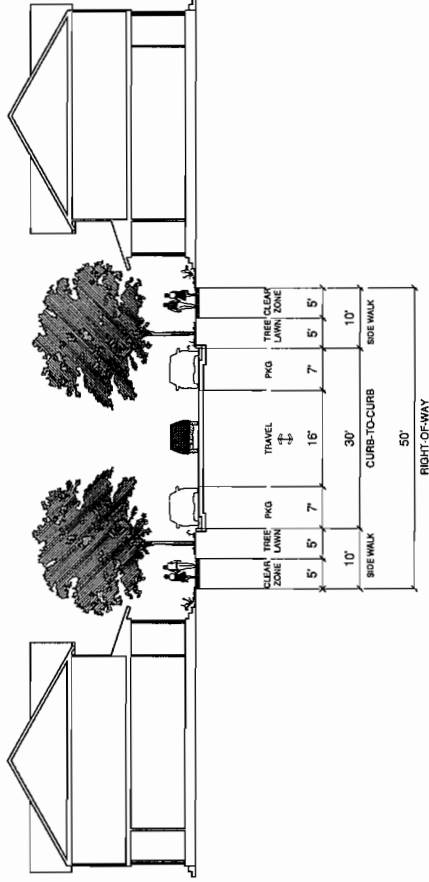
LAKELINE STATION
PUD
Exhibit G - Street Cross
Section Illustrations

Austin, Texas

April 06, 2007



E.
Local Street with
Driveways



LOCAL STREET WITH DRIVEWAYS

SUMMIT PROPERTIES
Irvine, California

LAKELINE STATION PUD Exhibit G - Street Cross Section Illustrations

Austin, Texas

April 06, 2007

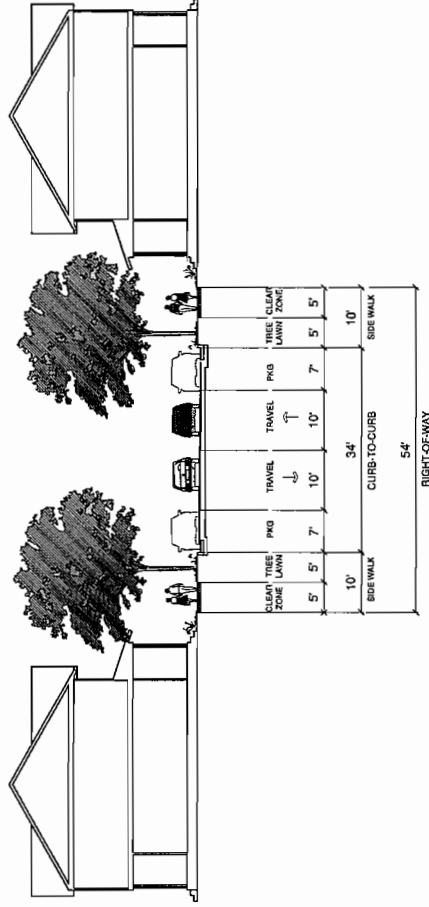


F. Local Street without Driveways

NOTE: This street section matches
"Mixed Residential Avenue" from the
City of Austin TND Standards.

SUMMIT PROPERTIES
Irvine, California

G-7



LOCAL STREET WITHOUT DRIVEWAYS

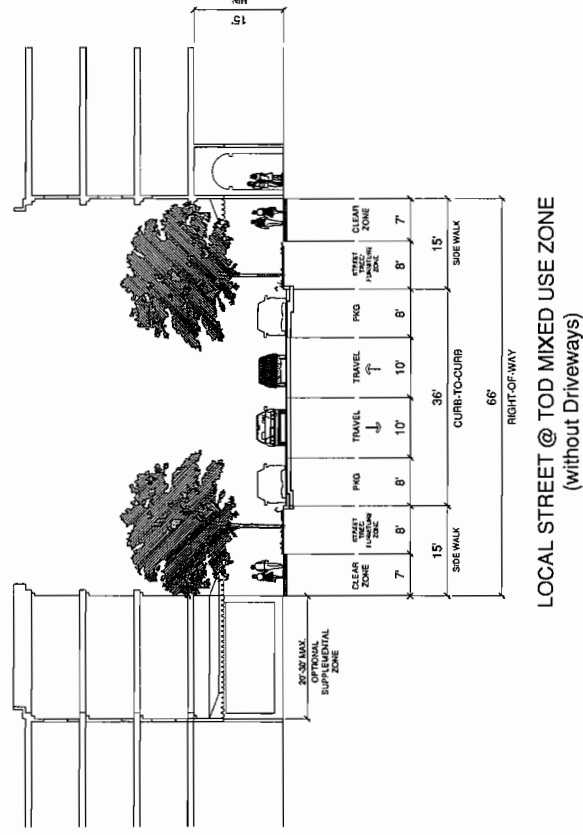
LAKELINE STATION
PUD
Exhibit G - Street Cross
Section Illustrations

Austin, Texas

April 06, 2007



G.
Local Street at
TOD Mixed Use Zone



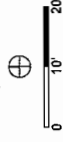
SUMMIT PROPERTIES
Irvine, California

G-8

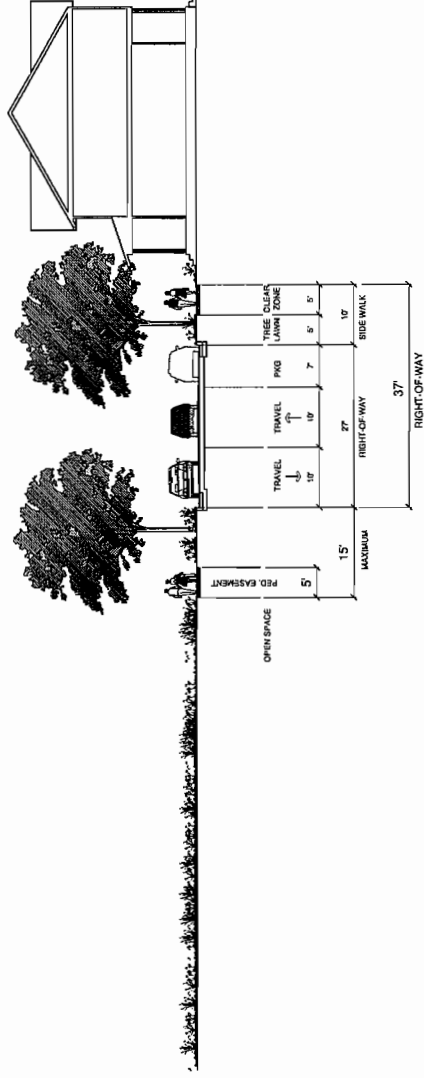
LAKELINE STATION PUD Exhibit G - Street Cross Section Illustrations

Austin, Texas

April 06, 2007



H. Local Street at Open Space



LOCAL STREET
(at Open Space)

NOTE: A minimum 5' wide perpetual public access easement shall be provided outside the street right-of-way on the open space side. The easement will be required at the time of subdivision and should not meander more than 15' from the right-of-way line and must intersect at street intersections.

SUMMIT PROPERTIES
Irvine, California

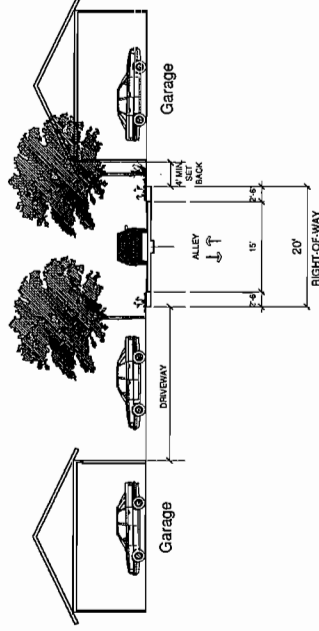
LAKELINE STATION PUD Exhibit G - Street Cross Section Illustrations

Austin, Texas

April 06, 2007



I. Residential Alley



TYPICAL RESIDENTIAL ALLEY
(20' Right-of-Way)

NOTE: This street section matches
"Mixed Residential Alley" from the
City of Austin TND Standards.

SUMMIT PROPERTIES
Irvine, California

**ENVIRONMENTAL
 FEATURES MAP**

**LAKELINE STATION PUD
 PACIFIC SUMMIT PARTNERS**

PROJECT NO: 1655-02
 DESIGNED BY: DAM
 CHECKED BY: PRO
 DATE PLOTTED: 06-08-07
 PLOTTING SCALE: 1" = 1'

SHEET
1
 OF
2

EXHIBIT H



NOTES
 1. BASED ON THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) FLOOD INSURANCE RATE MAP (FIRM) DATED JANUARY 3, 1993. NO PORTION OF THE SITE IS WITHIN THE 100-YEAR FLOODPLAIN.
 2. IMPERVIOUS COVER SUMMARY
 OVERALL SITE IMPERVIOUS COVER= 62.0%

PUD DISTRICT	AREA (AC)	COLLECTIVE IMPERVIOUS COVER (AC)
TOO MIXED USE ZONE	23.4	18.5
ATTACHED/CLUSTER RESIDENTIAL	77.5	65.7
SINGLE FAMILY RESIDENTIAL	140.2	102.4
PARKS/OPEN SPACE/RETENTION	62.5	3.7
OWC	12.3	6.0
TOTAL	316.9	196.3

3. CUT/FILL ALLOWANCES
 0' CUT/FILL - RESIDENTIAL
 10' CUT/FILL - MULTIFAMILY/COMMERCIAL AREAS
 20' CUT/FILL - PUD AREAS
 4. TREE SURVIVAL AS REQUIRED WILL BE DETERMINED DURING THE FINAL P-21

**EXHIBIT
 H**

Exhibit 'I'

**ENVIRONMENTAL AND WATER QUALITY
REGULATIONS**

FOR

LAKELINE STATION PUD

AUSTIN, WILLIAMSON COUNTY, TEXAS

Prepared for:

PACIFIC SUMMIT PARTNERS, LLC
111 Congress Avenue, Suite 1950
Austin, Texas 78701

Prepared by:

BURY + PARTNERS, INC.
221 West 6th Street, Suite 600
Austin, Texas 78701

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**JULY 2007
REVISED SEPTEMBER 2007**

EXHIBIT I

INTRODUCTION

The proposed Lakeline Station Planned Unit Development (PUD) is approximately 326.8 acres in size and is located near the northwest corner of Parmer Lane and RM620 within the City of Austin in Williamson County, Texas. The Lakeline Station PUD is a transit oriented development (TOD) located adjacent to Capital Metro's Northwest Park and Ride Station. The proposed PUD land use plan is included in the Appendix of this report as *Exhibit C*.

The Lakeline Station PUD measures and commitments to environment and water quality enhancements described below will insure that the proposed PUD will be an environmentally superior project to what could be achieved with existing ordinances.

CRITICAL ENVIRONMENTAL FEATURE PROTECTION

The proposed Lakeline Station PUD recognizes the existence of several Critical Environmental Features (CEF's) and will commit to the setbacks recommended by staff as presented in Table 1 and shown in *Exhibits H and H-1*. The following are the items that Lakeline Station PUD is committed to provide.

1. No residential lots may include a CEF or be located within 50 feet of a CEF.
2. Residential lots should not include any portion of a CEF buffer. Setback distance must be 150 feet radius unless stated otherwise in Critical Environmental Feature Summary Table (Table 1).

Feature ID	Feature Type	Buffer Zone	Feature Description
W-1	Wetland	150 ft or 1:1 mitigation	200'x400' man-made wetland along the southern tip of the site.
W-2	Wetland	50 ft from stream centerline	Northwest corner on the unnamed tributary to Brushy Creek
W-3	Wetland	150 ft	Northeast corner pond with extended periods of water retention
S-4 (aka COA 3)	Karst - Collapse Sinkhole	150 ft upslope; 150 ft in all other directions	Shallow, elongated sink hole depression (85'x20'x2' deep)
COA 16	Karst - Sinkhole	300 ft upslope; 75 ft south; 50 ft north	located in drainageway above stocktank (15'x12.5'x2.3' deep), northeast corner

Table 1. Lakeline Station PUD – Critical Environmental Feature Summary Table

3. No disturbance of native vegetation is allowed within the buffer zone.
4. No construction or placement of structures including buildings, sheds, pools, landscaping or gardens is allowed within a Critical Environmental Feature buffer zone.
5. Stormwater disposal or irrigation prohibited within a CEF buffer zone.
6. Erosion and sedimentation controls must be installed at the perimeter of all CEF buffers prior to the initiation of construction.
7. All CEF's and associated CEF buffers must be shown on all plats, preliminary plans, site plans and construction plans. Plat notes shall be included that include restrictions listed in 3 and 4 of this section.
8. CEF buffers adjacent to drainageways must be platted separately and not within drainage or utility easements. No utilities are allowed within CEF buffers.
9. Integrated Pest Management Plan and or CEF Operation and Maintenance Plan shall include section addressing the removal of nuisance vegetation (poison ivy, etc.) as identified in the plan.
10. Acceptable fencing shall be provided at the edge of all CEF buffer zones. The fencing shall be 6 feet high and contain a lockable access gate for each non-continuous buffer. The fencing shall be installed along the buffer edge prior to the initiation of construction.
11. Water quality BMP's should not drain to CEF buffer zones but should drain to areas where overland sheet flow may be maintained. Level spreaders or similar structures will be required for any discharges near CEF buffer zones.
12. An Operation and Maintenance Plan will be prepared for the long term management of all CEF buffers. Items covered with the Operation and Maintenance Plan will address trash removal, pet waste pickup, nuisance vegetation removal and inspections within the buffers. The acceptance of the Operation and Maintenance Plan shall be done prior to approval of a final plat section containing a CEF.
13. A restrictive covenant shall be entered into with the City to grant access to City of Austin staff to all CEF buffers within the Lakeline Station PUD.
14. Wastewater and stormwater utility lines constructed within 500 feet of wetlands may require flow retards or Controlled Low Strength Material (CLSM) as fill around manholes as a means of preventing interception of subsurface groundwater flow away from wetlands.

INTEGRATED PEST MANAGEMENT PLAN

An Integrated Pest Management (IPM) Plan shall be prepared in accordance with the Pollution Prevention Measures listed in the Environmental Criteria Manual Section 1.6.9.2.D.

GREEN BUILDER PROGRAM

The Lakeline Station PUD will achieve a two-star rating in Austin Energy's Green Building Program. This will apply to the entire PUD area recognizing that only a portion of the site is located within the Austin Energy service area.

WATER CONSERVATION

On August 24, 2006, the City of Austin Council passed Resolution #20060824-061 that created a taskforce with the goal of drafting a policy document consisting of strategies and implementation plans for reducing peak water use by 1% per year for 10 years. Lakeline Station PUD has recognized this future policy and has committed to implement the following strategies immediately.

1. Prohibit inefficient plumbing fixtures
2. Establish efficiency requirements for cooling tower management
3. Establish efficiency standard for commercial clothes washers
4. Limit frequency, timing, and method of outdoor watering
5. Require new residential irrigation systems to meet design standards and permitting requirements
6. Require homebuilders to offer a "WaterWise" landscape option
7. Require analysis of automatic irrigation systems
8. Require water audits for high-volume residential customers
9. Native and/or naturalized plants for landscaping in commercial areas

The environmental impacts to these strategies are seen in several aspects. First, the reduction in water use helps reduce the energy needed for water treatment and pumping and therefore increasing air quality. The landscaping and irrigation measures will help reduce the amount of runoff and also limit the need for lawn fertilizer/chemicals. Finally, these water conservation strategies will help keep lake levels higher which will help preserve wetland habitats.

WATER QUALITY ENHANCEMENTS

Use of Wet Ponds

Lakeline Station PUD will utilize wet ponds to handle water quality treatment as well as flood control detention. Wet ponds will be used where the drainage area criteria (ECM 1.6.6B) is satisfied.

Increased Capture Volume

The developer has proposed to provide additional stormwater capture volume over and above that currently required by Code. The developer is committed to a minimum capture volume equal to the runoff from the one-year, three-hour event for the purposes of water quality and streambank erosion control. The following is a table comparing the current City of Austin Code requirements with that required by the Lakeline Station PUD.

Impervious Cover	COA Capture Volume (inches)	Required Capture Volume (inches) ¹	Increase
40%	0.70	0.75	7%
50%	0.80	0.92	15%
60%	0.90	1.08	20%
62%	0.92	1.11	21%
70%	1.00	1.24	24%
80%	1.10	1.41	28%
90%	1.20	1.57	31%
100%	1.30	1.74	34%

¹ Source: LCRA Nonpoint Source Pollution Control Technical Manual, July 2007

Lakeline Station has proposed an overall impervious cover of approximately 62% and the following table presents the treated volume for each development district in comparison to the volume currently required by City of Austin Code. The volumes associated with each development district are shown for comparison purposes as the various ponds on the site will handle portions of several districts. Each volume is presented with 100% of the district draining to the control with the exception of the Parks District. Actual volumes will be calculated as described in ECM, Section 1.6.2

Development District	COA Water Quality Volume (cubic feet)	PUD Water Quality Volume (cubic feet)	Increase
TOD Mixed Use Zone	92,786	112,287	21.0%
Attached/Cluster Residential	331,426	399,234	20.5%
Single Family Residential	549,875	659,850	20.0%
Parks/Open Space/Detention	53,807	53,807	0%
Civic	35,131	40,944	16.5%

Rainwater Harvesting

In an effort to provide additional controls for water quality, the developer has proposed the use of rainwater harvesting for 100% of the commercial use buildings within the Lakeline Station PUD. The system shall be designed to accept the water quality volume within seventy-two (72) hours after the end of the rainfall event and to detain and treat the water quality volume in accordance with the PUD requirements. The collected water may be used to irrigate landscaped or natural areas on the site. Irrigation systems shall be designed in accordance with standard irrigation practices considering such factors as soil type slope, and vegetation.

**LAKELINE STATION PUD
EXHIBIT F - CODE MODIFICATIONS**

CODE REFERENCE	COMMENT/PROPOSED ACTION
LDC 25-2 ZONING	
25-2-243	<p>Requires that district boundaries for zoning or rezoning applications must be contiguous.</p> <p>Proposed Action: Delete the requirement.</p> <p>Justification of Proposal: The subject PUD zoning application is for a transit oriented development, which includes the dedicated extension of Lakeline Boulevard. More appropriate TOD planning principles may be utilized by including the property on both sides of Lakeline Boulevard within the land use plan.</p>
25-2-411(H)(3)	<p>Provides that for multifamily residential uses, a land use plan must include a maximum floor to area ratio.</p> <p>Proposed Action: Delete the requirement.</p> <p>Justification of Proposal: Since the PUD has a mix of residential and commercial uses within each development district, maximum floor to area ratios were provided for each of the development districts, instead of for only multifamily residential uses.</p>
25-2-411(I)(4)(a)	<p>Provides that for non-residential uses, a land use plan must include (a) 25 feet for a front yard, and (b) 15 feet for a street side yard.</p> <p>Proposed Action: Delete the requirement.</p> <p>Justification of Proposal: The front yard and street side yard setbacks were reduced to more appropriately accommodate transit oriented development. Section 25-2-766.13 of the LDC outlines site development regulations within the City's defined TOD districts. This code section states that maximum front yard and street side yard setbacks should be 15 feet, which acknowledges that setbacks within transit oriented developments should be minimal.</p>

Exhibit J

CODE REFERENCE	COMMENT/PROPOSED ACTION
25-2-411(I)(5)	<p>Provides that for non-residential uses, a land use plan must include the number of curb cuts or driveways, which must be the minimum necessary for adequate access to the site.</p> <p>Proposed Action: Delete the requirement.</p> <p>Justification of Proposal: Based on the size of the PUD, there will be a network of streets, which will affect the number of driveways. Commercial sites are required to submit site plans for review and approval by the City, which means that the number of driveways can be evaluated at that time. The site development standards provided in the PUD for commercial sites in the TOD Mixed Use district requires parking garages, which will limit the number of driveways.</p>
25-2-411(K)	<p>Provides for open space requirements for certain tracts.</p> <p>Proposed Action: Delete the provision for open space requirements to apply to individual tracts within the PUD .</p> <p>Justification of Proposal: The land use plan shows open space for the entire PUD that exceeds 20 percent (63.5 acres out of 316.9 net site area.) There is an additional 9.9 acres of open space in the CWQZ and WQTZ that is not included in the calculation. However, it is not clear if this code section refers to individual tracts within the PUD, as well as the overall PUD. While the project meets the overall 20 percent open space requirement, individual tracts within the PUD will not meet this requirement. Individual tracts within the PUD that are closest to the Northwest Park and Ride will have less than 20 percent open space to facilitate density for successful rail usage.</p>
25-2-766.22(B)(7)	<p>Provides for a housing affordability analysis and feasibility review for achieving specific affordable housing goals.</p> <p>Proposed Action: Delete this requirement.</p> <p>Justification for Proposal: Instead of performing a housing affordability analysis, the Applicant has agreed, separate from the zoning case, to have ten percent of the residential units serve households at 80 percent of the median family income.</p>

CODE REFERENCE	COMMENT/PROPOSED ACTION
<p>25-2 Subchapter C, Article 10 (Compatibility Standards)</p>	<p>Provides for compatibility standards of conflicting uses both internal and external to the site.</p> <p>Proposed Action: Delete the requirements of Subchapter C, Article 10 within the Lakeline Station PUD and replace with modified compatibility standards described below.</p> <p>A. SF-5 and more restrictive zoning districts/land uses located outside of the Lakeline Station PUD may trigger compatibility standards, as outlined within Subchapter C, Article 10 of the City Code, to all property within the PUD, except for the TOD Mixed Use district.</p> <p>B. Within the boundaries of the Lakeline Station PUD, compatibility standards as outlined in Chapter 25-2, Subchapter C, Article 10 of the Code do not apply to the TOD Mixed Use district.</p> <p>C. Within the boundaries of the Lakeline Station PUD, compatibility standards as outlined in Chapter 25-2, Subchapter C, Article 10 of the Code apply to the Attached/Cluster Residential/Mixed Use and Single Family Residential/Mixed Use districts, except as modified below:</p> <ol style="list-style-type: none"> 1. Compatibility standards shall not apply between residential uses. 2. Section 25-2-1052, Exceptions, shall apply. 3. Compatibility standards applicable to commercial and other permitted uses: <ol style="list-style-type: none"> a. Section 25-2-1065(A), Scale and Clustering Requirements, shall apply. b. A 25-foot vegetative buffer area, measured from a property line of a residential use to a building, shall be provided and maintained between residential uses to the following commercial and civic uses: automotive rentals, business or trade school, business support services, cocktail lounge, commercial off-street parking, consumer repair services, convenience storage, food preparation, hotel-motel, indoor entertainment, indoor sports and recreation, intensive recreational facilities, off-site accessory parking, outdoor entertainment, plant nursery, printing and publishing, service station, software development, theater, veterinary services, drive through services, and custom manufacturing. <p>(i) Mixed-use buildings are exempt from this requirement. A mixed-use building is a building containing any mix of commercial/office/live-work/residential units.</p> <p>(ii) Improvements permitted within the 25 foot vegetative buffer zone are as</p>

CODE REFERENCE	COMMENT/ PROPOSED ACTION
	<p>follows: hike and bike trails, pedestrian trails, sidewalks, solid fences, detention, water quality and drainage facilities consisting primarily of earthen structures, utility improvements, overhead electric or communication lines, or those improvements that may otherwise be required by the City of Austin or specifically authorized in this ordinance.</p> <p>c. A 100 foot wide buffer area, measured from the property line of a residential use to a building developed with industrial, commercial, or civic uses shall be provided and maintained.</p> <p>(i) Mixed-use buildings are exempt from this requirement. A mixed-use building is a building containing any mix of commercial/office/live-work/residential units.</p> <p>(ii) Improvements permitted within the 100 foot buffer zone are as follows: hike and bike trails, pedestrian trails, sidewalks, solid fences, detention, water quality and drainage facilities consisting primarily of earthen structures, underground utility improvements, overhead electric or communication lines, streets, alleys, driveways, surface parking facilities limited to 12 spaces or fewer, or those improvements that may otherwise be required by the City of Austin or specifically authorized in this ordinance.</p> <p>d. Section 25-2-1064, Front Setback, shall not apply.</p> <p>e. Height restrictions are listed as follows:</p> <p>(i) Within 50 feet of an undeveloped platted residential lot (measured to the nearest building setback line on the property), a commercial building may not exceed three stories or 40 feet in height.</p> <p>(ii) Within 50 feet of a developed residential building, a commercial building may not exceed two stories or 30 feet above the height of the existing residential use.</p> <p>(iii) Beyond 50 feet from a developed residential building, a commercial building may be constructed to the permitted height as set forth in this PUD.</p> <p>f. Except for lighting in a public right-of-way, all exterior lighting must be hooded or shielded so that the light source is not directly visible from adjacent properties. Exterior lighting may not exceed 0.25 foot candles across the source property line.</p> <p>g. The noise level of mechanical equipment may not exceed 60 decibels at the property line.</p>

CODE REFERENCE	COMMENT/ PROPOSED ACTION
	<p>h. The use of highly reflective surfaces, including reflective glass and reflective metal roofs with a pitch of more than a run of seven to a rise of 12, is prohibited. This prohibition does not apply to solar panels and copper or painted metal roofs.</p> <p>i. Dumpsters and permanently placed refuse receptacles must be located a minimum of 25 feet from adjacent residential uses. The location of and access to dumpsters or any other refuse receptacles must comply with the Transportation Criteria Manual.</p> <p>j. Commercial and Civic uses must be screened in accordance with this subsection. Yards, fences, vegetative screening, or berms shall be provided to screen off-street parking areas containing 12 or more parking spaces on a single site, mechanical equipment, storage areas, and areas for refuse collection. If fences are used for screening, the height may not exceed six feet unless otherwise permitted in the Land Development Code. The property owner is responsible for the upkeep and maintenance of fences, berms, and vegetative screening.</p> <p>Justification of Proposal: Not requiring compatibility standards within the TOD Mixed Use district is consistent with Section 25-2-1052(F) of the Code which states that property within TOD districts are excepted from compatibility standards once a Station Area Plan is adopted. Compatibility standards throughout the remainder of the PUD should be limited, in order to provide the additional density needed to support the TOD area and to encourage neighborhood services in these areas.</p>
25-2-1006(C)	<p>Provides for screening of specific uses as designated in the Environmental Criteria Manual.</p> <p>Proposed Action: Delete the requirements of this section within the Lakeline Station PUD because of the mixed-use nature of the project.</p> <p>Justification for Proposal: Removing the screening requirement allows the mix of use desired for the project and the natural connection of the multiple districts.</p> <p>Screening should not be necessary between buildings within the PUD, since there are no parking lots within the project. The purpose of a TOD is to construct a pedestrian friendly</p>

CODE REFERENCE	COMMENT/ PROPOSED ACTION
	<p>environment. Screening between buildings and uses creates a hostile environment and barriers for pedestrians.</p> <p>In addition, within the Attached/Cluster Residential/Mixed Use and Single Family Residential/Mixed Use districts, there are no buildings that are 100 percent commercial. Commercial uses are located within live-work units, which are townhome type of buildings. Between the TOD Mixed Use and Attached/Cluster Residential/Mixed Use districts, there should be a seamless progression between the buildings.</p>
<p>25-2 Subchapter E, Design Standards and Mixed Use</p>	<p>Provides design standards for commercial and mixed use projects.</p> <p>Proposed Action: Delete the requirements of this section within the Lakeline Station PUD.</p> <p>Justification for Proposal: The design standards proposed within the PUD are more complimentary to a TOD development than the suburban roadway standards required in 25-2, Subchapter E, Design Standards and Mixed Use portion of the code.</p>
<p>LDC 25-4: SUBDIVISION</p>	
<p>25-4-171</p>	<p>Provides for each lot in a subdivision to abut a dedicated public street.</p> <p>Proposed Action: Modify this requirement for access to lots so that 1) each lot in a subdivision, except a lot that fronts on a plaza or a green court and abuts an alley, shall abut a dedicated public street and so that 2) direct vehicular access from a single family residential lot to an alley is permitted and preferred. Lots containing multifamily, condominium, mixed-use, commercial and civic uses do not require direct vehicular access from a lot to an alley.</p> <p>Justification for Proposal: Except for the uses noted above, direct vehicular access from a lot to a street is not permitted except where (a) if a lot does not abut and take direct access to an alley, or (b) the director determines it is warranted by exceptional circumstances. If the adjacent lots have direct vehicular access to a street, the Director may require that the access be through a common or joint driveway. Notwithstanding the above, direct</p>

CODE REFERENCE	COMMENT/ PROPOSED ACTION
	vehicular access from a single-family residential lot to a street is permitted provided that (a) access is taken to a street with a standard cross section and (b) driveway separation requirements are met. Design criteria for SF-1/SF-2 sized local street standards may be utilized when it can be demonstrated that significant on-street parking will not occur or that off-street parking comparable to SF-2 is utilized.
25-4-154	<p>Provides for streets, alleys and pedestrian paths to be designed and constructed in accordance with the Transportation Criteria Manual.</p> <p>Proposed Action: Modify this requirement for street design and construction so that streets, alleys and pedestrian paths may be designed and constructed in accordance with the Traditional Neighborhood Criteria Manual, the Standards Criteria Manual, the Standard Specifications Criteria Manual or attached Exhibit G, Street Standards and Parking document.</p>
LDC 25-6: TRANSPORTATION	
25-6-171	<p>Describes standards for the design and construction of streets.</p> <p>Proposed Action: Delete this requirement.</p> <p>Justification for Proposal: It was replaced with a modified Section 25-3-52(I), as stated in this Exhibit.</p>
25-6-172	<p>Describes requirements for arterial streets.</p> <p>Proposed Action: The attached Exhibit G, Street Standards and Parking document shall govern.</p> <p>Justification for Proposal: The proposed street sections for this transit oriented development have been designed with specific criteria relative to current planning principles to create a pedestrian friendly and urban environment.</p>

CODE REFERENCE	COMMENT/PROPOSED ACTION
25-6-173	<p>Describes requirements for collector streets.</p> <p>Proposed Action: The attached Exhibit G, Street Standards and Parking document shall govern.</p> <p>Justification for Proposal: The proposed street sections for this transit oriented development have been designed with specific criteria relative to current planning principles to create a pedestrian friendly and urban environment.</p>
25-6-292(A)	<p>Sets standards for design, construction, alteration, or repair of sidewalks, driveway approaches, pavement, appurtenances on public property, or other facilities to provide access to adjoining property.</p> <p>Proposed Action: Add language "...except as modified by zoning."</p> <p>Justification for Proposal: The intent is to acknowledge that the other changes made to 25-6 and the TCM affect this provision.</p>
25-6-321	<p>Specifies that the Council or Director may require an existing driveway to conform with requirements in Article 5 and the Transportation Criteria Manual</p> <p>Proposed Action: Add language "...except as modified by zoning."</p> <p>Justification for Proposal: The intent is to acknowledge that the other changes made to 25-6 and the TCM affect this provision.</p>
25-6 Article 7: Off Street Parking and Loading	<p>Provides general regulations and design and construction standards for off-street parking and loading.</p> <p>Proposed Action: Delete entire section and replace with Exhibit G, Street Standards and Parking document.</p> <p>Justification for Proposal: The proposed parking requirements for this transit oriented development have been designed with specific criteria relative to current planning principles to create a pedestrian friendly and urban environment.</p>

CODE REFERENCE	COMMENT/ PROPOSED ACTION
25-6 Appendix A	<p>Provides minimum parking and loading requirements for motor vehicles and bicycles.</p> <p>Proposed Action: Delete entire section and replace with Exhibit G, Street Standards and Parking document.</p> <p>Justification for Proposal: The proposed parking and loading requirements for this transit oriented development have been designed with specific criteria relative to current planning principles to create a pedestrian friendly and urban environment.</p>
LDC 25-8: ENVIRONMENTAL	
25-8-64	<p>Provides impervious cover calculations for duplex or single-family lots.</p> <p>Proposed Action: Delete this entire section and replace it with the PUD requirement in Part 8 of the ordinance which would require a preliminary plan be submitted to allocate impervious cover on a per lot basis.</p> <p>Justification for Proposal: The preliminary plan would allocate impervious cover to residential, commercial, civic, and park/open space lots ensure that the maximum impervious cover allowances outlined in the PUD are not exceeded.</p>
25-8-322	<p>Describes clearing limits for a roadway.</p> <p>Proposed Action: Revise to allow clearing of up to twice the ROW width.</p> <p>Justification for Proposal: The type of residential product being contemplated within the Lakeline Station PUD may require a larger clearing area for the roadway to allow for garage access and to provide the proper drainage for the block.</p>

CODE REFERENCE	COMMENT/PROPOSED ACTION
25-8-341	<p>Describes requirements for cuts on a tract.</p> <p>Proposed Action: Revised to allow cuts in accordance with Exhibit H, Environmental Feature Exhibit.</p> <p>Justification for Proposal: The increased cut allowances are requested for the following reasons: 1) the use of wet ponds and the extra depth necessary for these facilities, 2) the product type building contemplated with the Attached/Cluster Residential and TOD/Mixed Use Districts, and 3) the overall site because of predominant alley loaded blocks.</p>
25-8-342	<p>Describes requirements for fills on a tract.</p> <p>Proposed Action: Revised to allow fills in accordance with Exhibit H, Environmental Feature Exhibit.</p> <p>Justification for Proposal: The increased fill allowances are requested for the following reasons: 1) the use of wet ponds and the extra depth necessary for these facilities, 2) the product type building contemplated with the Attached/Cluster Residential and TOD/Mixed Use Districts, and 3) the overall site because of predominant alley loaded blocks.</p>
25-8-394 (B)	<p>Provides for impervious cover limitations in the upland zone of the Brushy and Lake Creek Watersheds.</p> <p>Proposed Action: Modify to match Exhibit D and Environmental Feature Exhibit H. In addition modify section (1)(a) to 60% for single family residential with lots greater than 5,750 square feet. Modify section (2) to state that the maximum overall impervious cover shall not exceed 64% for the development including open space, detention, civic, right-of-ways, single family residential, multi-family residential, and commercial uses. Delete sections (3) and (4).</p> <p>Justification for Proposal: This development is adjacent to the future Lakeline Station and in an effort to follow desires of City staff, the overall density is much higher around the station than normal commercial / residential projects. By allowing an overall impervious cover limitation, the project is able to develop at the desired densities immediately surrounding the station. To offset the increased impervious cover modification for the</p>

CODE REFERENCE	COMMENT/ PROPOSED ACTION
	residential and commercial elements of the project, water quality permanent structures will include wet ponds providing increased TSS reduction to the sand filter basins.
25-10: SIGN REGULATIONS	
25-10-154	<p>Provides for regulations for subdivision identification signs.</p> <p>Proposed Action: Major entrances for the subdivision will be identified by the roadway classification intersecting the Lakeline couplet. A 'connector' road, as defined within this PUD, at the Lakeline couplet will be considered a major entrance to the subdivision.</p>