



MEMORANDUM REVISED

To: Jeff Howard
McLean & Howard, LLP

Date: June 28, 2016

Project: The Grove At Shoal
Creek

CC: Andrew Linseisen, P.E.
Development Services Department

Sherri Serwaitis
Planning and Zoning Department

From: Austin Transportation Department

Re: Review Comments

The Austin Transportation Department has reviewed the March 28, 2016 (received June 16, 2016) traffic report regarding the *"The Grove at Shoal Creek, Traffic Impact Analysis"*, prepared by R-K Traffic Engineering, LLC. The proposal calls for constructing 110 Single Family Homes, a 600 unit apartment building, 425 condo/townhouse dwelling units, a 600 room congregate care facility, 225,000 SF of office, 55,000 SF of shopping center, a 35,000 SF supermarket, plus additional uses. The development would be constructed between Bull Creek Road, Shoal Creek and 45th street. The following comments summarize our review findings:

Unresolved Traffic Impact Analysis (TIA) Comments

Analysis Comments

1. The 2018 analysis, as presented in the TIA, does not include the following:
 - Full build out of the Bull Creek Road and West 45th Street intersection
 - The improvements at the Bull Creek Road/Driveway 1 intersection
 - The improvements at the Bull Creek Road/Jackson Avenue intersection
 - The improvements at the Mopac/45th Street intersection
 - The improvements at driveways 2 through 5 along Bull Creek Road

This analysis was however included in the 2024 analysis. Based on the information provided in the current revision of the TIA, ATD understands that these intersection improvements will be fully built out prior to completion of Phase 1 of the development (see other comments below). Please clarify if otherwise.

2. **Repeat Comment ATD7 from March 2016:** It appears from the information provided in the TIA that 14% of the site generated volumes will use Jackson



Avenue. This site generated traffic will significantly increase traffic volume on Jackson Avenue. However, mitigation has not been proposed along Jackson Avenue to address this increase in traffic. We recommend that when a signal warrant study is conducted by the Applicant for the signal at Jackson Avenue and Bull Creek Road, the Applicant also study Jackson Avenue to determine whether mitigation is needed address the increase in traffic.

Geometric Comments

The Applicant will include design plans addressing these geometric comments, and those addressed by the ATD memorandum dated March 28, 2016 as part of the site plans:

Bull Creek Road/West 45th Street Intersection Plan – Preferred Option 2:

1. ATD had conceptually accepted the concept plan (Option 2) at the intersection of 45 Street/ Bull Creek Road, submitted by the Applicant, dated December 15, 2015 (as per Transmittal, dated March 25, 2016).

ATD recommends that acquisition of all necessary ROW (as proposed in the Plan – Option 2 submitted by the Applicant) and construction of the intersection at 45 Street / Bull Creek Road according to the plan be one of the conditions of approval of the PUD.

ATD also recommends that the Applicant provide documentation that this, and all other ROW, has been obtained to allow construction of the proposed improvements at this location as proposed.

2. The northbound right turn is too narrow to allow for a WB-50 design vehicle to make the turn. The lane should be widened by shifting the outermost curb and not the island curb line.
3. The northern curb face of the pork-chop island must be offset by two (2) feet from the travel lane for eastbound traffic.
4. On the eastbound approach, the 100 feet approach taper is insufficient in length. The taper should be lengthened by narrowing the painted island.
5. The concept plan shows four (4) feet wide sidewalk on the northwest of the intersection along 45th Street. All sidewalks must be minimum five (5) feet wide.

Bull Creek Road Improvements Plan (comments start at the north and head south):

1. It is unclear at this time if sufficient ROW will be obtained for the proposed improvements along Bull Creek Road. In addition, since there are a number of comments regarding the proposed design along Bull Creek Road, it is unclear if



the total ROW needed has been adequately identified, particularly at the PHB locations and the traffic signal at Jackson Avenue. If this ROW is not obtained there is concern that the proposed improvements along Bull Creek Road will not be able to be constructed.

ATD requests that the Applicant provide verification that the required ROW along Bull Creek Road, has been dedicated/obtained to allow construction of the proposed improvements at this location as proposed.

2. Tapers shown between the back-to-back turn lanes are insufficient in length. A single taper between the two turn lanes should be provided.
3. The PHB, crosswalks and landings are not shown at Driveway 1. Please show this information.
4. The 185 feet taper on the northbound left turn approach to Jackson Avenue is insufficient in length. Lengthen the taper and narrow the painted island.
5. The traffic signal, crosswalks and landings are not shown at Driveway 2/Jackson Avenue. In addition, no information is presented on Jackson Avenue related to length of turn lanes and tapers. Please present this information.
6. Between Driveway 5 and Driveway 4, the Applicant is proposing a 10-foot wide southbound lane, 11-foot wide lane northbound with a 9-foot wide shoulder. ATD recommends that the Applicant provide 10-foot wide travel lanes including a center two-way left-turn lane.
7. The pedestrian refuge island shown at Driveway 4 does not appear to have offsets to the travel lanes as provided. We recommend that one foot (1') minimum offsets be provided.
8. The PHB, crosswalks and landings are not presented at Driveway 4 in the concept plan. Please present this information.
9. The 167' lane taper south of Driveway 4 appears to be too short. In addition, it is unclear how the improvements south of Driveway 4 will match the existing conditions, including how the existing northbound bicycle lane will transition onto the multi-use path. Please present this information.
10. It is unclear from the information contained in the TIA as to when the concrete safety barrier for the bicycle lane will be constructed along Bull Creek Road. The Applicant has indicated in conversations with ATD that the barrier will be installed when Bull Creek Road is reconstructed to provide the other proposed improvements listed in the TIA. The Applicant will include design plans of this barrier installation with the site plans for the development.



Vehicular Connection to 45th Street from Jackson Avenue Extension

1. The Applicant provided traffic analysis for this proposed connection and included it in Appendix J of the TIA. However, the applicant didn't model full connection of Jackson Avenue from Bull Creek Road to 45th Street in Synchro. Also the TIA did not document how the diversion of the site trips and additional diverted trips (if any) were determined. We recommend that the Applicant review and provide justification of the diverted site trips and any additional diverted trips.
2. The site plan must include the proposed layout and cross section for the Jackson Avenue Extension from Bull Creek Road to West 45th Street. At the connection to West 45th Street, the cross section of Jackson Avenue should be wide enough to accommodate emergency vehicles. Bicycles and pedestrians should be accommodated as part of the complete streets policy.
3. Since no internal plans have been provided for the Jackson Avenue Extension from Bull Creek Road to 45th Street, we recommend that as part of the site plans for the development this roadway (called a driveway in the TIA) be designed such that a consistent cross-section, with bike lanes and sidewalks is provided between Bull Creek Road and 45th Street. In addition, we recommend that the design speed of this new roadway connection be 30 mph.
4. It is Austin Transportation Department's understanding that the Jackson Avenue Extension connection from Bull Creek Road to 45th Street shall be fully funded by the Applicant, including the PHB, as part of the improvements during the implementation of the 2018 improvements.
5. The Austin Transportation Department understands that the Applicant has purchased 2627 45th Street for ROW and additional ROW is being pursued along 45th Street which will be provided for this connection. Austin Transportation Department also understands that movements at this "new" intersection will be restricted to right in/right out only. Plans will need to show how turning movements will be restricted and which design vehicles can be accommodated. ATD requests that the applicant submit plans presenting these details at this proposed connection. If the additional ROW is not obtained we recommend that this access be limited to right-out only.



6. Advisory Comment: ATD had significant comments on the preliminary plan(s) previously submitted for this proposed new access (please submit plans as per comments 2, 3, and 4 above). The comments on the previously submitted plans are as follow:
 - a. The proposed splitter island is shown as 20.5' along 45th Street. This distance is insufficient to prevent vehicles from making an illegal left into the site or an illegal through movement from the site to Chiappero Trail. We recommend that the island be enlarged to prevent these movements.
 - b. The proposed splitter island is proposed to be constructed with type 1 mountable curb. We recommend that the island be constructed with non-mountable curb to prevent illegal movements.
 - c. The lanes on either side of the splitter island appear to be approximately 12'. We recommend that these lanes be widened to accommodate, at a minimum, a fire truck.
 - d. The Pedestrian Hybrid Beacon Signal on the west side of the proposed driveway is too close to the stop bar. This needs to be a minimum of 40' from the stop bar to allow for sight distance. We recommend that the design be modified to meet proper sight distance.

Development Phasing Comments

1. Based on the analysis presented in the TIA, all the improvements need to be constructed in 2018. The Applicant is requesting that these improvements be constructed when Phase 1 development reaches 2,000 vehicle trips per day. These improvements must be constructed when either the 110 single-family homes and half of the residential condominiums (188 units) or when all the residential condominiums (375 units) are complete. These intensities equate to the approximately 2,000 vehicle trips per day requested. It is our understanding that no construction on-site will occur beyond these units until all the improvements identified in the TIA for 2018 are complete. We recommend that these thresholds and restrictions be included in the Final TIA memorandum prepared by DSD and be one of the conditions of approval of the PUD.

Staff will conduct further review of the subject application with regard to geometric constraints that may arise due to inadequate or unavailable right-of-way that may affect the operational objectives of proposed infrastructure improvements. These elements may affect site plan review and approval as they are considered integral to the viability of the subject development as proposed.

PART XX. The Grove at Bull Creek PUD Affordable Housing Program.

A. In order to meet the City's affordable housing goals and to ensure long-term affordability, the Landowner and the Landowner's successors and assigns (collectively referred to as the "Landowner") agree to the following:

1. Ten percent of the total number of multifamily rental housing units located within the Grove at Bull Creek PUD will be set aside for occupancy by households with incomes at 60 percent of or below the median family income (each an "Affordable Rental Unit," collective "Affordable Rental Units") in the Austin metropolitan statistical area for a rental affordability period of forty years (collectively, the "Rental Affordability Requirement") from the date of a certificate of occupancy. In addition the Landowner agrees to comply with the following:
 - a) The Rental Affordability Requirement period for each multifamily development with Affordable Rental Units (the "Affordable Development") begins on the date a final certificate of occupancy is issued for each Affordable Development.
 - b) Affordable Rental Units must contain a product unit mix of studio, one, two and three bedroom units in accordance with Fair Housing Laws.
 - c) Each lot or site sold or developed for use as an Affordable Development shall be subject to a restrictive covenant using the form shown in Exhibit XX (subject to revision) or agreed upon by the Director of Neighborhood Housing and Community Development (NHCD) and Landowner at the time of the sale or development and recorded in the official public records of the county where the Affordable Development is located.

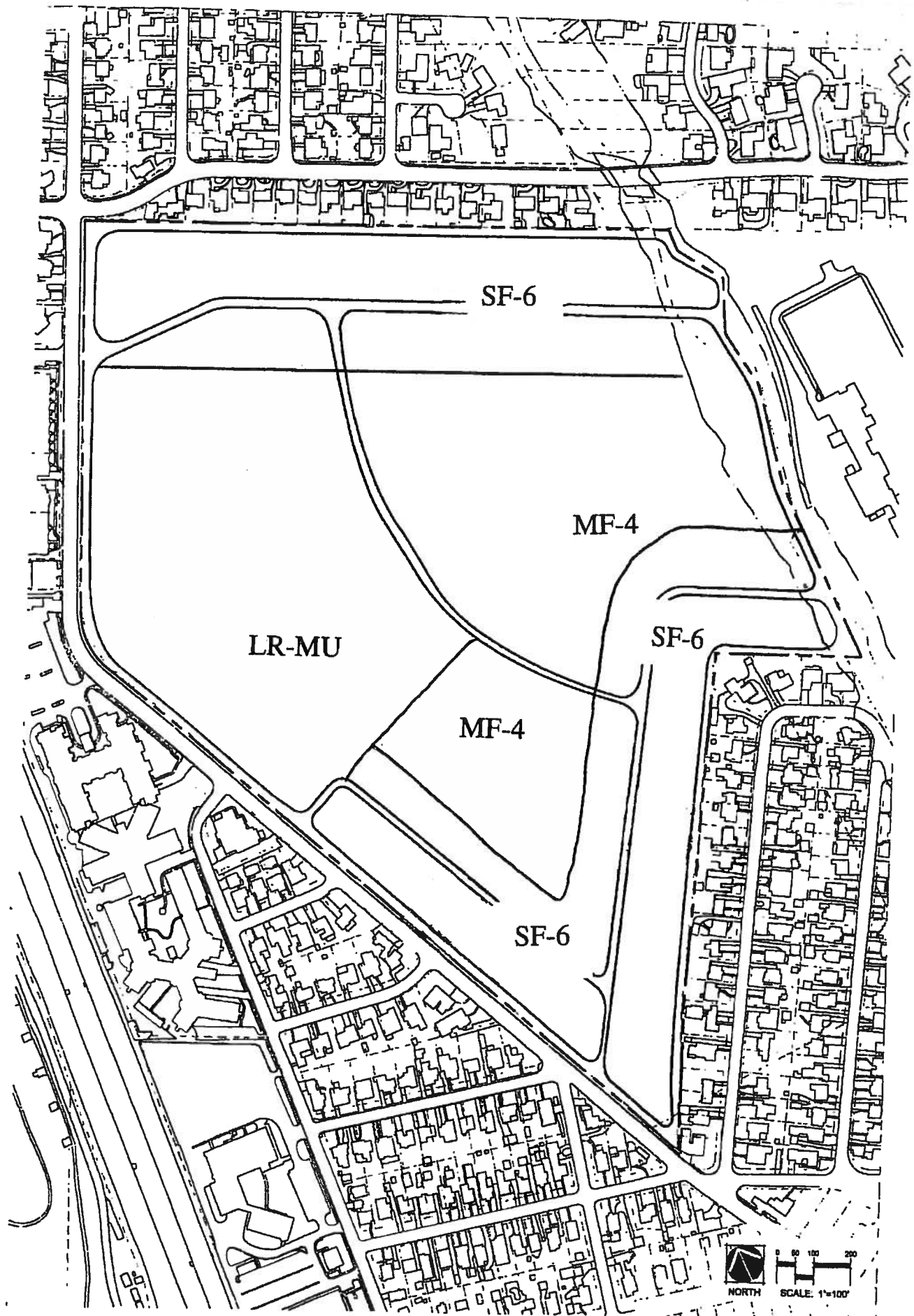
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B. At least 5 percent of the total number of units sold as owner-occupied residential housing units located within the Grove at Bull Creek PUD will, through a mechanism agreed upon by the City and Landowner, be made permanently available at a price affordable to households with incomes at 80 percent of or below the median family income (each an "Affordable Ownership Unit," collective "Affordable Ownership Units") in the Austin metropolitan statistical area (collectively, the "Ownership Affordability Requirement").

1. The Affordable Ownership Units constructed on any site shall have substantially similar architectural design and restrictions as other residential units offered for sale to the general public on such site.
2. The Affordable Ownership Units must contain a product unit mix of studio, one, two and three bedroom units deemed feasible by Developer; provided that, however, no fewer than 50% of the Affordable Ownership Units shall have at least 2 bedrooms.
3. Affordable Ownership units must:

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- a) Be sold to an income eligible household at 80 percent of or below median family income;
 - b) Include resale restrictions that require that resale of the affordable unit must be to a household at 80 percent of or below median family income; and
 - c) Contain restrictions that will cap the equity gain to the homeowner that can be realized upon resale of the affordable unit. The resale formula will be set by the director of the Neighborhood Housing and Community Development Department, and may change from time to time; and
 - d) Contain a Right of First Refusal to the Austin Housing Finance Corporation (AHFC) or other entity designated by the City that is assignable to an income-qualified buyer, to ensure long term affordability.
- C. The Landowner agrees to enter into an agreement with the City of Austin that ensures compliance with Part XX of this PUD ordinance.
- D. Income limits for the Affordable Housing Requirements shall be established annually as determined by the United States Department of Housing and Urban Development.
- E. The Landowner shall file a written report with the Director on the number and location of each Affordable Ownership Unit and Affordable Rental Unit meeting the Affordable Housing Requirements within the Grove at Bull Creek PUD (the "Affordability Report"). The initial Affordability Report shall be filed within 15 calendar days following the March 31 or September 30 next following the date of recordation of a plat or site plan within the Grove at Bull Creek PUD and be continuously filed on a semi-annual basis until the project is fully built out and sold.
- F. Compliance with the Affordable Housing Requirements will be monitored by the City's Neighborhood Housing and Community Development Department through an annual audit of the sale and rental of Affordable Ownership Units and Affordable Rental Units within the Grove at Bull Creek PUD. Income qualifications, rents and sales price of the ownership units must comply with NHCD compliance guidelines.
- G. Compliance with this Part XX of this PUD Ordinance satisfies the requirements of Section 25-1-704(B)(2)(a) of the Austin City Code so that development within The Grove at Shoal Creek PUD shall be eligible for a waiver of 100% of the development fees, including capital recovery fees, described in Section 25-1-704 and Section 25-9-397 of the Austin City Code as allowed by and in accordance with the City's SMART Housing Program.



The Grove at Shoal Creek Planned Unit Development

C814-2015-0074

April 4, 2016

List of Superiority Items:

Parks Superiority

- 1) The project exceeds the minimum amount of parkland by at least 1.55 acres. This is clearly superior to the minimum code requirements for parkland dedication as this represents a 13.66% increase above code requirements.
- 2) The developer will spend \$1 million for the improvements to the Parks within the project whereas there is no code requirement for any monies to be spent on park improvements. This represents a 100% increase over Code requirements.
- 3) The developer will perpetually maintain the parkland. The estimate is over \$200,000 per year and this is at no cost to the City of Austin or the taxpayers. This represents a 100% increase over Code requirements.

Transportation Superiority

- 1) Funding and Constructing all mitigation measures identified for Bull Creek Road. Mitigation measures include additional auxiliary lanes at Jackson Avenue and other Site Driveways, widening of Bull Creek Road between Driveway 1 and 45th Street, and the dedication of ROW from the subject Site to construct these improvements.
- 2) Funding and Constructing 100% of Intersection Improvements for 45th and Bull Creek when pro-rata share is 26.5%. Improvements will include eastbound and westbound left turn lanes on 45th Street, additional northbound left turn lane on Bull Creek Road, and additional northbound right turn lane on Bull Creek Road as well as improved pedestrian crossings and reconstruction of sidewalk at all four corners of the intersection.
- 3) Providing trail connectivity to Ridglea Greenbelt.
- 4) Constructing 12-foot Shared Use Path along Bull Creek Road.

- 5) Constructing 12-foot Shared Use Path along 45th Street Greenbelt.
- 6) Constructing protected southbound Bike Lane on Bull Creek Road in front of Site.
- 7) Installation of Pedestrian Hybrid Beacons on Bull Creek Road and 45th Street to facilitate pedestrian connectivity.
- 8) Post fiscal for, and if easement obtained, construction of Bike and Pedestrian Bridge across Shoal Creek and trail connection from Bridge to Shoal Creek Blvd.
- 9) Bike lanes on major internal street cross-sections.
- 10) Installation of racks to park 400 bicycles.
- 11) Require shower facilities in offices to help facilitate bicycle commuters.
- 12) Contribution of \$100k in seed money for neighborhood multi-modal improvements.

Environmental Superiority

- 1) The project will substantially exceed open space requirements (by more than 50%) and will cluster development away from Shoal Creek.
- 2) The project will provide water quality controls through the use of a wet pond and/or green infrastructure for a minimum of 50% of the required water quality volume on-site.
- 3) The project will provide that a minimum of 10 acres of impervious cover on the site will drain to and be substantially treated by Green Water Quality Controls such as rain gardens and biofiltration facilities.
- 4) The project will not modify the existing 100-year floodplain.
- 5) The project will preserve a minimum of 75% of protected quality native tree inches on site.
- 6) The project will provide a tree care plan for all preserved protected and heritage trees on site.

- 7) The project will direct stormwater runoff from impervious surfaces to a landscaped area at least equal to the total required landscape area.
- 8) The project will provide flood mitigation for the 9.39 acres of existing impervious cover on the site which is not currently detained. The existing State office development does not have any detention or flood control facilities and current code does not require new development to mitigate for existing impervious cover. If the project participates in the RSMP, the RSMP fee will be paid as if the current 9.39 acres of impervious cover does not exist.
- 9) The project will provide educational signage at the Wetland CEF.
- 10) The project will provide minimum 3" caliper street trees on all internal streets. Under conventional zoning, Tracts A, C, D, and E would be residential zoning districts and would not require street trees under Subchapter E. This area of additional street trees represents 34.24 acres or approximately 45% of the total project area.
- 11) The project will provide street trees along Bull Creek Road where they would not be required by Subchapter E.
- 12) The Project will provide an Integrated Pest Management Plan for all sites.
- 13) The Project will commit to 95% of non-turf plant species from Grow Green or equivalent per the Design Guidelines.
- 14) The project will provide additional protection for the $\frac{3}{4}$ critical root zone for all protected and heritage trees within the Signature Grove, which includes all trees around the proposed pond and the highest quality oak trees throughout the Signature Park.

Affordable Housing

- 1) At least 10% of the rental units affordable to 60% MFI or less with long term affordability provided.
- 2) At least 5% of for sale units affordable to 80% or less with long term affordability.



Don Gardner
Consulting Arborist

Registered Consulting Arborist #438
Certified Arborist TX0228

**PECANS ON
THE GROVE SITE**

November 7, 2015

For: AGR Bull Creek

I examined all protected trees on this entire site in February and March, 2015.

The magnificent live oak grove is a natural wonder. How can trees this old be so healthy and vigorous? Now, the job is to make sure they stay healthy. Protecting 3/4 of the critical root zone, which I've heard is proposed, is one of the ways to do that.

Most of the pecans on the site, however, are an altogether different story. They are all planted pecan varieties, not native pecans.

Orchardists have been developing new pecan varieties for decades. None are as good a tree (i.e., hardy, strong structure, long-lasting) as the old, original native varieties. Some varieties are better than others, but some are truly horrendous and should have never gotten out into the landscape market.

These varieties are not good, strong pecan trees, but are weak with multiple structural issues. They have more in common with Arizona ash trees, with notorious flaws, than with native pecan trees.

The health of a tree can go bad, or the structure can go bad.

The structural issues with the pecans in question include, 1) poor branch attachment, 2) co-dominant trunks with one trunk growing laterally, not upward, and 3) included bark that holds water, and decays easily and rapidly.

In addition, pecans have the reputation for branch breakage, which they deserve. And, it can be counted on, the newer varieties will break when they get a little size and weight on them.

Further, due to prolonged heat and drought, many of the pecans at this site, like pecans all over central Texas, have major crown dieback, with all the decay and structural issues that come with large dead wood.

Someone planted poor pecan varieties and they lasted a while. But now, the trees are coming apart. They should be removed.

Unfortunately, the pecans are not good, strong, long-lived natives like the live oaks in the groove.

Please contact me if you have any questions.

A handwritten signature in black ink, appearing to read 'Don Gardner', with a stylized, cursive script.

Don Gardner, RCA
Registered Consulting Arborist #438
Certified Arborist TX 0228



Don Gardner
Consulting Arborist

Registered Consulting Arborist #438
Certified Arborist TX0228

**TREE CARE
RECOMMENDATIONS
FOR
LIVE OAK GROVE**

**ON FORMER
TX DOT PROPERTY
LOCATED NEAR THE
INTERSECTION OF
BULL CREEK ROAD AND 45TH STREET**

SPRING, 2015

**PRODUCED FOR
MILESTONE BUILDERS
NORRIS DESIGN**

**Produced By
Don Gardner, RCA
Registered Consulting Arborist #438
Certified Arborist TX0228**

LEGEND FOR ABBREVIATIONS USED IN THE FIELD REPORT FORMS

By Don Gardner, RCA

- RV: Remove volunteers. The areas around and between the trees has been mowed using a tractor and shredder for many years. Fortunately, the mowers stayed away from the bases of trees and did not regularly hit them, which is often the case. However, in the spaces between the mowed areas and the tree trunk bases, many young sapling trees have grown. They range from one-half inch to three inches in diameter. In many instances there are so many volunteer trees and shrubs around the bases, one can barely get to the tree. The volunteer tree root systems are becoming interwoven with the large tree's root system. All of these volunteers must be removed. This work must be done carefully and sensitively.
- DW: Prune dead wood two inches in diameter and larger. Pruning large dead and decayed wood minimizes and prevents larger decay in trees, prevents many structural problems from happening, and preserves the tree many more years. Proper and skilled pruning, according to International Society of Arboriculture standards, is critical to preserving trees.
- Rep.: Repair is a pruning term that means to prune broken branches (usually caused by high winds) properly so the wound will close and decay will be minimized.
- Train: When tree crowns grow into each other, become tangled and branches interwoven, training prunes to separate the trees so they don't constantly rub wounds and cause broken and dead branches.
- Vines: Wild grape vines and poison ivy vines are not good for trees. They eventually shade out and break canopy branches.

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3211	C elm						RV = remove vegetation RV, DW, repair, remove low limbs around base
3212	LO						DW, repair remove characteristics
3213	LO						RV, close by
3215	LO						RV, DW, Repair
3214	C elm						RV DW, repair
3608	LO				Tree 3/5m 1 tree		RV Repair ends
3609	LO				low 2/4 possible		RV Repair DW, leave
3216	LO						RV DW, repair
3926	LO						RV, DW, Rep
3217	LO						RV DW, rep.
3218	LO						RV DW, rep.
3925	LO						RV

Tree ID	Tree Species	Vigor	Structure	Observations	Maint. Task	Maint. Priority	Tree Comment
3927	LO						Remove. Trunk lateral
3928	elm.						and ladder often
3219	LO				DW, repair, RV-13ep		RV, DW, rep
3220	LO						RV, DW, rep
3233	LO						RV, DW, rep, remove wire,
3234	LO						RV, DW, rep
3235	LO						RV, DW, rep
3236	LO						RV, DW, rep, poison ivy
3238	LO						RV, DW, rep, remove
3237	LO						RV, DW, rep, vines
3241	LO						RV, DW, rep, vines
3242	LO						RV, DW, rep.
3239	LO						RV, DW, rep, train
3240	LO						RV, DW, rep, train
3244	LO						RV, DW, rep, train
3245	LO						DW, Rep, train
3251	LO						RV, DW, Rep, vines

Page 3

1=dead or dying 2=poor 3=fair 4=good

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3252	LO						RV, DW, Rep, Vines, train
3253	LO						RV, DW, Rep, Vines, train
3254	LO						RV, DW, Rep, train
3255	LO						RV, DW, rep
3246	LO	✓	1 Crown				RV, DW, rep, train
3247	LO	✓					RV, DW, rep, train
3248	LO						RV, DW, rep, train
3249	LO						RV, DW, rep, train
3250	LO						RV, DW, rep, train
3224	LO						RV, DW, rep, Vines
3225	LO						RV, DW, rep, train, Vines
3226	LO						RV, DW, rep, train

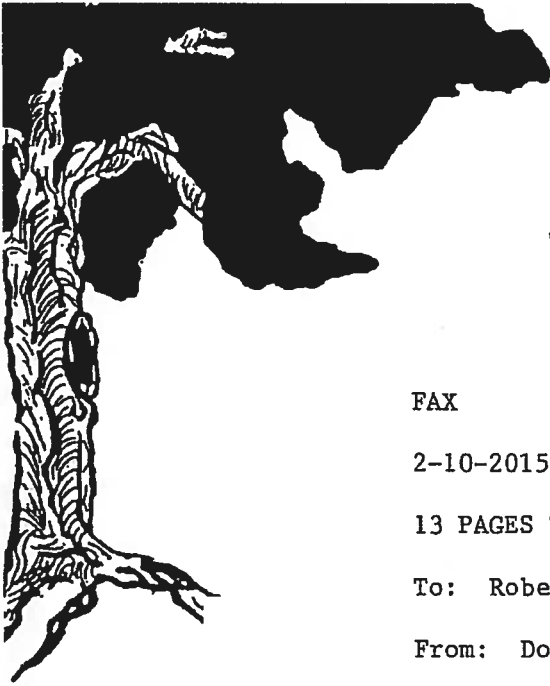
P4

1=dead or dying 2-poor 3-fair 4-good

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3221	LO						Dead - remove
3220	LO						RV, DW, rep
3222	LO						RV, DW, rep
3223	LO						RV, DW, rep, vines
3257	LO						RV, DW, rep, vines
3246	elm						Remove - center leader broken
3228	LO						RV, DW, Rep
3229	LO						RV, DW, Rep
3230	LO						RV, DW, Rep
3227	elm						RV, DW, Rep
3258	LO						RV, DW, Rep
3259	LO						RV, DW, Rep

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3260	LO						RV, DW, Rep
3261	LO						RV, DW, Rep
3262	LO						RV, DW, Rep
3265	LO						RV, DW, Repair, Veneer
3266	LO						RV, DW, Repair, Veneer
3263	LO						RV, DW, Repair
3264	LO						RV, DW, Repair
3268	LO						RV, DW, Rep
3269	LO						RV, DW, Repair, Veneer
3267	Dead						RV, DW
3295	LO						Remove broken branches RV, DW, repair to
3293	C. Jun						Dead crown MT - remove to protect

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3294	LO						RV, DW, Rep, Vains
3287	LO		no tree top				RV, DW, Rep, Vains elevated on right edge of street
3298	LO						
3299	—		not here				
3292	LO						RV, DW, Rep, Vains
3290	LO						RV, DW, Rep, Vains
3288	LO						RV, DW, Rep, Vains
3289	LO						RV, DW, Rep, Vains
3291	LO						RV, DW, Rep, Vains
3300	LO						DW, Rep



Don Gardner
Consulting Arborist

Registered Consulting Arborist #438
Certified Arborist TX0228

FAX

2-10-2015

13 PAGES TOTAL

To: Robert Deegan

From: Don Gardner, Consulting Arborist

Enclosed please find the 12 pages of ^{completed} evaluation forms
for the Milestone project.

Bull Creek Project 2-4-75

P1

1=dead or dying 2=poor 3=fair 4=good

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment	2-4-75
3065	Western wood			2			2 nichia contacted base decay pockets	
3064	live oak			3/4			15" high surface rot, from headwound planted 20-inches	
3085	Tallow			3			ca tallow protected?	
3084	Tallow			3	DN repair			
3083	Tallow			3	"			
3082	C. elm			3	"		18" - not 20"	
3081	Hock honey			2			trunk decay/branch decay/ mistletoe	
3080	ant elm			2			CO-dom, hypox, crown 400g gone wet wood, decay pockets	
3067	Pear			3	DN repair			
3066	Tallow 33"			3	"			
3073	Lygus multi-trunk							
3072	dead cottonwood			1			16' protected	

42

1=dead or dying 2=poor 3=fair 4=good

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3071	Crape myrtle			3			18.5 inches
3070	"			3			22 inches
3068	Pecan			3/4	DW repair		
3069	Black holly			2			multiple co-doms, very weak structure
3073	Pecan			3	DW repair		27 inches
3076	Pecan			3	"		unusual form. low stem goes 45° - Over extended
3074	Pecan			2			18" Very bad structure Weak. Trunk Trunk will break
3077	Pecan			2/3			27" Been de-horned
3078	Pecan			3			Over extended branch
3079	Pecan			3			Over extended branch

2-16-15

live oaks not on tree list

2-4-15 P3

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Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3203	live oak (LO)			3			
3204	"			3			
3285	"			3			
3270	"			4			
3281	"			4			
3280	"			3			old lightning strike on fire scar
3277	"			3			
3278	"			4			
3279	"			3			
3271	"			3			fire scar
3272	"			4			
3273	"			3			fire scar + 90° trunk bend

2-4-15 P4

1=dead or dying 2=poor 3=fair 4=good

[illegible]

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3205	C. elm			2			Crown filled w/ mistletoe
3206	"			2			stem removed crown dropped much mistletoe + dead wood 3 scaffolds - 1 w/ much wood + dead Crown weak in rest
3207	Pecan			2			19" hypox weak crown
3208	Pecan			2/3			multiple co-doms / dead + broken branches. perhaps mitigate
3209	Pecan			3			
3210	Pecan			3			26" Central leader
3211	Pecan			2			26" w/ hypox low
3212	Pecan			2/3			26" Crown dieback + only extended branches

1=dead or dying 2=poor 3=fair 4=good

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3211	C elm			2/3			much mistletoe + old storm breaker
3212	L0			3			31"
3213	L0			3			18.5"
3215	L0			3			lightning strike clearing
3214	C elm			3/4			great tree
3216	L0			3			28" Big storm break
3218	L0			3			
3217	L0			3/4			30" FAB
3925	16" L0			3			
3926	18" L0			3/4			
3220	L0			3/4			Grand
3233	L0			3/4			33" old primary

P.7

1=dead or dying 2=poor 3=fair 4=good

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3234	LO			3/4			
3235	LO			3			
3238	LO			3/4			Twin trunks 40+'' dbh 1 trunk has entirely rotten
3237	LO			3/4			25''
3243	LO			3			need pruning / scaffold breaks
3239	LO			3			''
3240	LO			3			
3241	LO			3/4			
3242	LO			3/4			
3236	LO			3			FAB
3226	LO			3/4			25'' side stem doesn't count in dbh
3225	LO			4			Twin trunks 50''+ dbh FAB

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3255	LO			3			22" side stem doesn't count
3257	LO			3/4			3 Trunks 60" grand
3277	C elm			2	poor form		low scaffold broken lots low scaffold decaying main leader broken at 12' & decaying.
3280	C elm			2			
3221	LO			1			
3219	LO			3/4			old pile near cavity closing
3210	LO			3			Big scaffold breaks
3222	LO			3			old pile near cavity
3223	LO			3/4			35"
3228	LO			3			not counting big lateral 30"4 Crown stem breaks
3229	LO			3/4			
3230	LO			3/4			

1=dead or dying 2=poor 3=fair 4=good									
Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment		
3224	L0			3					
3244	L0			3/4					
3251	L0			4/3			Primary scaffold broken		
3252	L0			3/4					
3253	L0			3					
3245	L0			4			Grand		
3247	L0			3/4					
3246	L0			3/4					
3248	L0			3/4			3 together are grand		
3249	L0			3/4					
3250	L0			3/4					
3254	L0			3/4					

2-6-15

PE

1=dead or dying 2=poor 3=fair 4=good 2-9-15 P:10

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3232	Persea			2/3			Potom Aug! poor structure 26' bad co-dom - broken crown branched
3231	Persea			2			18'-19" active decay at co-dom
3258	LO			3			
3259	LO			3			2 trunks 50+ inches 18-19" persea live
3265	LO			3			
3266	LO			3			
3263	LO			3			
3262	LO			3			
3261	LO			3			
3260	LO			3			
3264	LO			3			
3267	Cedron (Persea)			3			

(Persea)

P11

1=dead or dying 2=poor 3=fair 4=good

2-9-75

Tree ID	Tree Species	Vigor	Structure	RATING 1-4	Maint. Task	Maint. Priority	Tree Comment
3296	Quercus ferrug			2			
3297	"			2			
3100	"			2			DN, decay
3200	LO			3			
3298	LO			3			poor long
3298	LO			3			
3289	LO			3			stump fore it up
3299	LO			1-2			
3290	LO			3			
3291	LO			3			
3292	LO						35" Homestead
noted tag 3287	LO						40" Homestead Camp

2-9-12

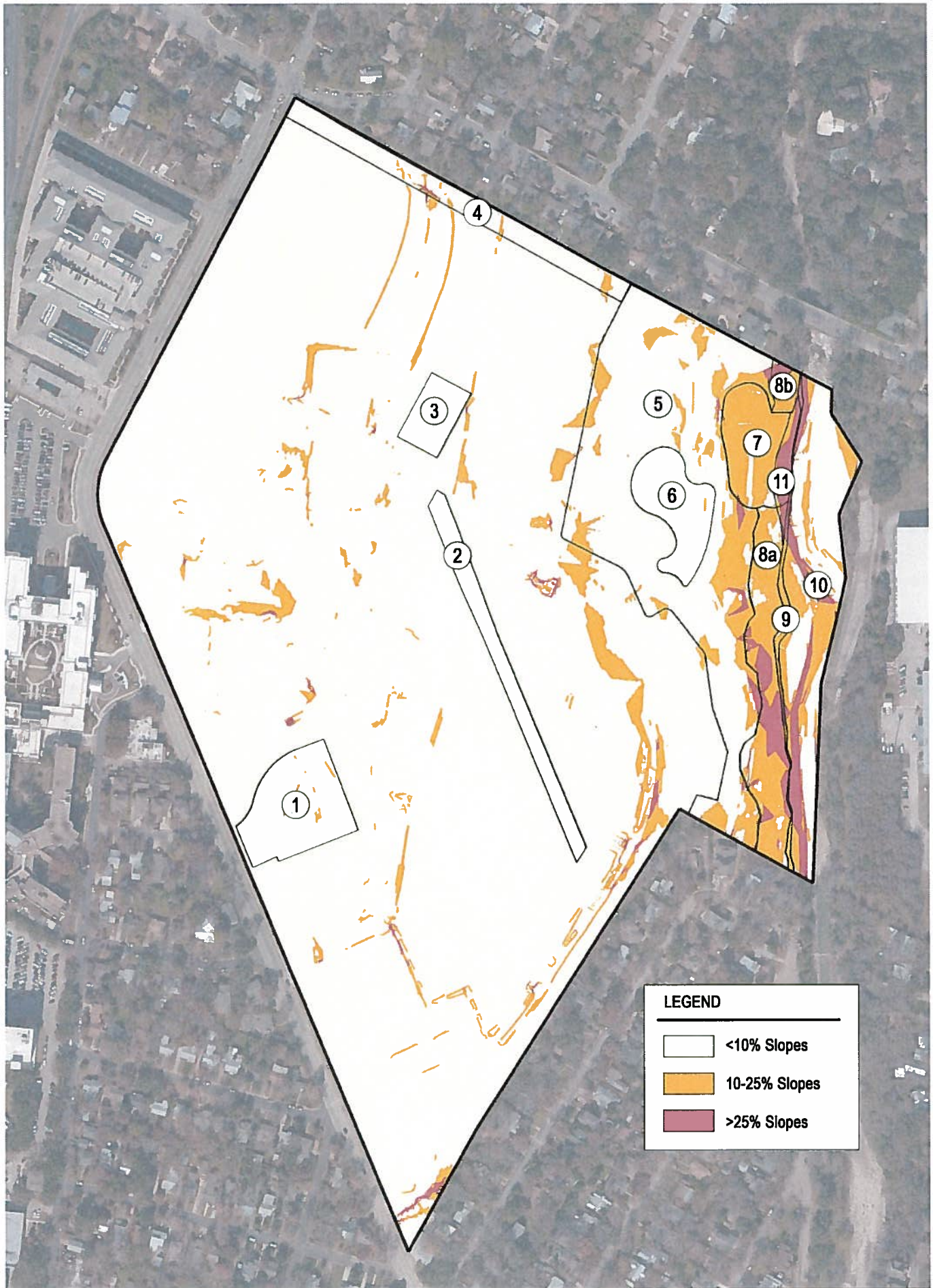
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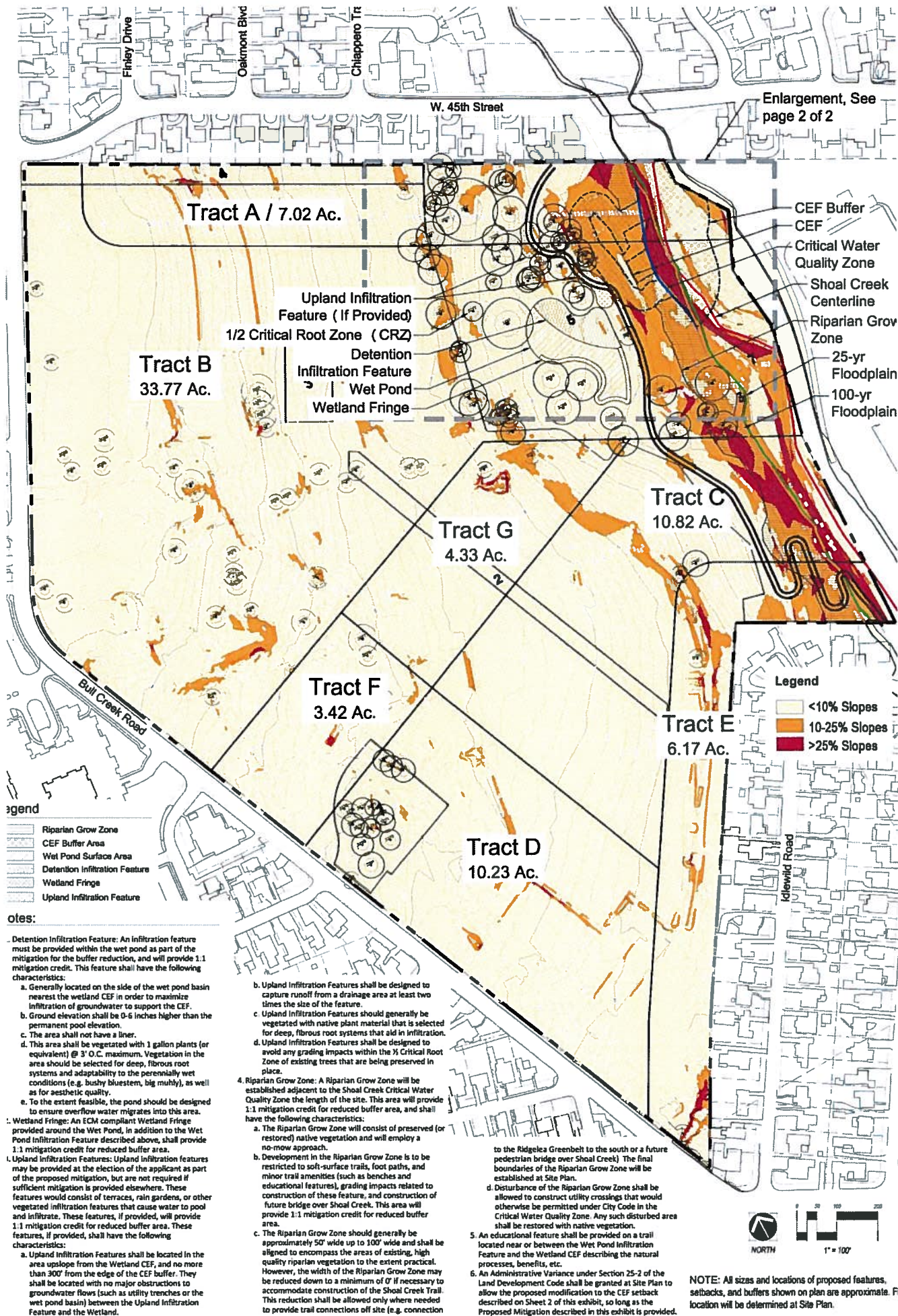
Parkland Slope Analysis
The Grove at Shoal Creek
February 15, 2016

Map Code	Park Description	Acreage of Park in Slope Category			Total Park Acreage
		<10% Slope	10-25% Slope	>25% Slope or Floodplain	
1	Neighborhood Park	1.22	0.03	0.00	1.25
2	Central Greenbelt	1.00	0.00	0.00	1.00
3	Plaza	0.50	0.00	0.00	0.50
4	North Greenbelt	0.78	0.08	0.01	0.87
5	Signature Park	3.77	1.64	0.13	5.54
6	Wet Pond	0.00	0.00	1.00	1.00
7	CEF and Buffer	0.00	0.00	0.94	0.94
8	Grow Zone	0.16	1.24	0.44	1.84
9	Signature Park - 100 yr FP	0.00	0.00	0.23	0.23
10	Signature Park - 25 yr FP	0.00	0.00	3.20	3.20
11	CWQZ (Outside FP)	0.00	0.00	0.24	0.24
NA	Flex Park Space	1.46	0.38	0.16	2.00
	TOTAL ACREAGE	8.89	3.38	6.35	18.62

Description	Acres	Percent of Requirement
Total Required Park Space	12.88	100%
Total Provided Park Space	18.62	145%
Total Credited Parkland	12.88	100%
Total Park Space under 10% slopes	8.89	69%



Parkland Slope Analysis
The Grove at Shoal Creek
February 17, 2016



The Grove at Shoal Creek Planned Unit Development Environmental Resource Exhibit



The Grove at Shoal Creek

Design Guidelines

October 24, 2016

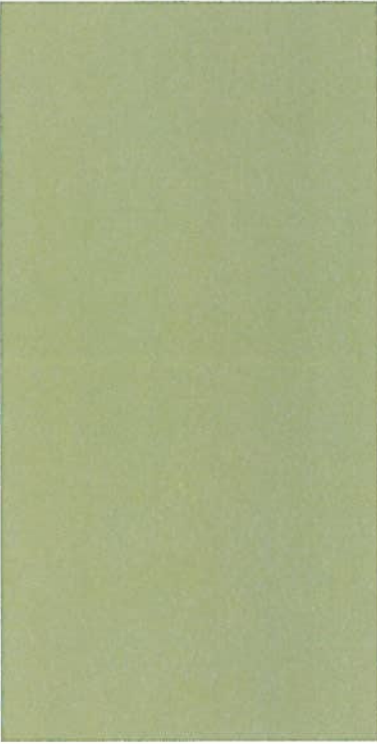






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1.0 BACKGROUND

1.1 The Site

The site for The Grove at Shoal Creek is an approximate 76-acre parcel in north-central Austin. The property was owned by TxDOT until ARG Bull Creek acquired the site in early 2015 and initiated the planning process that resulted in the Master Plan, Planned Unit Development (PUD) Zoning, and this document.

The Grove at Shoal Creek is surrounded by existing and established Central Austin neighborhoods including Allandale and Shoalmont to the north, Rosedale to the east, Ridgelea and Oakmont Heights to the south, and Westminster and the Post West Austin Apartments to the west. The site is accessed by Bull Creek Road to the west and 45th Street to the North, with the Mopac Freeway located just one block west of the site. There are existing CapMetro bus routes on Bull Creek Road with stops adjacent to the site. There are existing bike lanes on Bull Creek Road which are planned for improvement. The Shoal Creek Trail is also planned to eventually extend up to the project site along Shoal Creek.

Topography on the site falls from west to east, with the west end of the site occupied by existing 1-story office buildings and parking areas as well as relatively flat, undeveloped land. The east end of the site has slightly higher gradients and is dominated by a large grove of heritage oak trees as well as Shoal Creek, which is the site's eastern boundary. About 3.5 acres along Shoal Creek are in the 100-year floodplain.

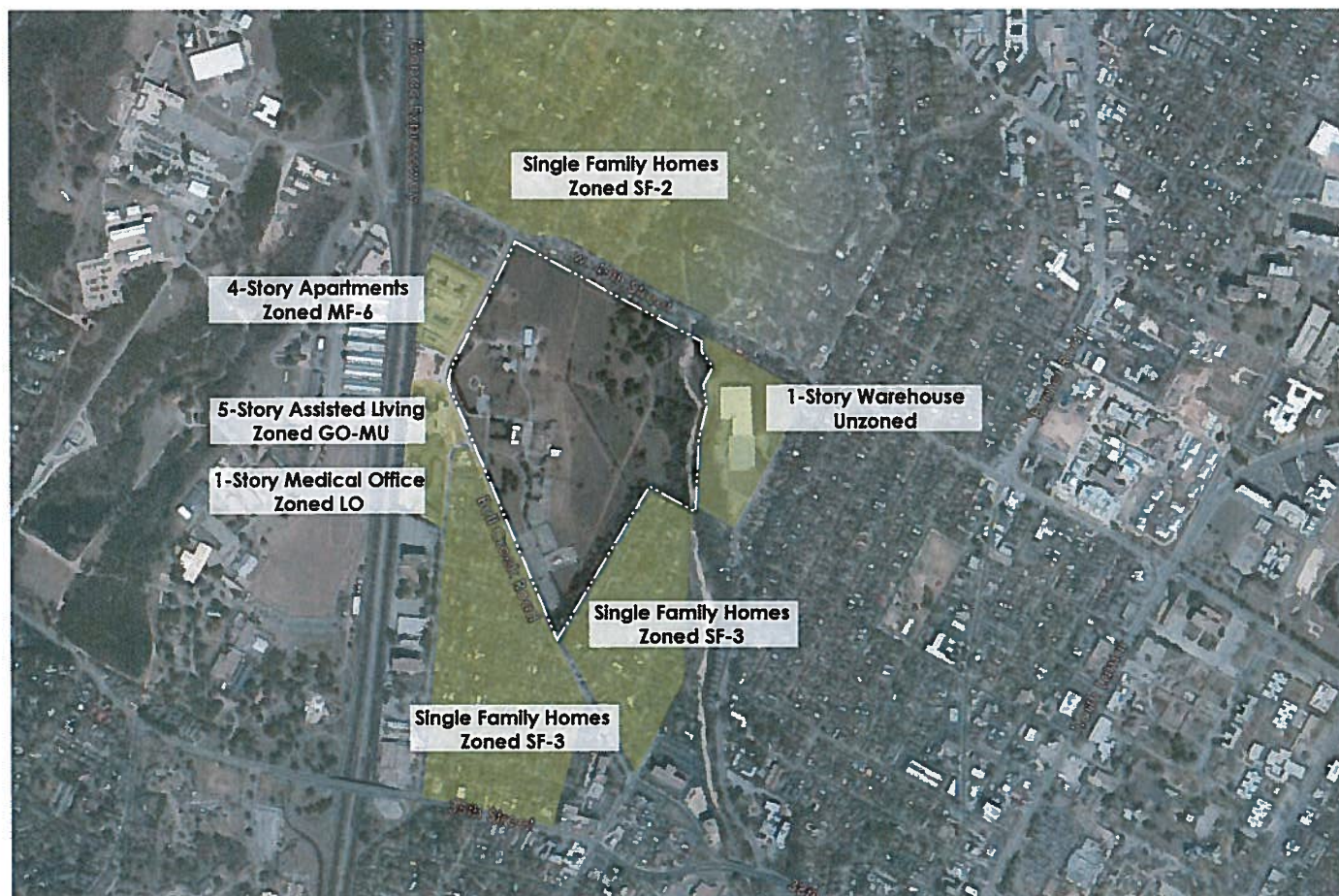


Figure 1.1: Context Map

1.2 Project History

ARG Bull Creek has put considerable effort into building a shared vision for The Grove at Shoal Creek through an inclusive and productive process. To achieve that vision, the project team has conducted an energetic community outreach program for The Grove.

The process began in January 2015 by surveying residents in Allandale, Bryker Woods, Highland Park West/Balcones, Oakmont Heights, Ridgelea, Rosedale, and Westminster at two workshops, and also online. Approximately 216 surveys were collected at the workshops, and 488 were taken online for a total of 704 surveys. Using these survey results, the team developed a vision for The Grove that reflects the community's collective vision in terms of its residential and commercial character, open space and the density of the development.

The team also engaged in dozens of formal meetings with various community groups and neighborhood leaders. Among these groups is the Bull Creek Road Coalition, a group formed in 2012 to help craft a vision and voice for sustainable development on the land ARG Bull Creek purchased from the State of Texas in late 2014, as well as the more recently formed Friends of The Grove.

The effort to publicize these meetings, events, and activities has included hundreds of signs and thousands of pieces of direct mail. The team has also maintained a website (www.TheGroveAtShoalCreek.com).



1.3 Using This Document

This document sets forth Design Guidelines for the design of the built environment within The Grove at Shoal Creek and is incorporated as part of the Planned Unit Development with the City of Austin. These guidelines are intended to supplement the zoning provisions of The Grove at Shoal Creek Planned Unit Development (PUD), which was adopted by the City of Austin on XXXX. The Design Guidelines will be administered by the City of Austin, through the Site Development Permit review process, and are subject to the final recommendations of the Traffic Impact Analysis (TIA).

The Design Guidelines are not intended to be prescriptive solutions that dictate a particular style, but rather performance criteria that can encourage diversity, creativity, and innovation within the framework established here. The Design Guidelines are organized into 5 Sections. This, the first Section, provides an introduction to the property and the document. Section 2 provides an overview of the overall vision and PUD Requirements. Section 3 establishes a framework of streets and transportation corridors that will form the structure of the community. This is the foundation off of which Sections 4 and 5 are built, and many of the guidelines in the other Sections are provided in relation to the framework established in Section 3. Section 4 establishes the architectural character of the community that will develop within that framework, and Section 5 establishes the landscape and open space character and establishes guidelines for lighting and signage. The NACTO Urban Street Design Guide was used as a reference in formulating the street designs in these guidelines and may be a useful guide in determining appropriate solutions for conditions not specifically addressed here.

2.0 THE PLAN

2.1 Vision

The Grove at Shoal Creek is envisioned as a legacy-quality neighborhood and model for innovative mixed-use urban infill development. This vision of community has three primary components: equity, economy, and ecology:



Build Bull Creek as a legacy-quality neighborhood;
a model for sustainable and innovative mixed-use
urban infill development.

Figure 2.1: Vision Diagram

2.2 Guiding Principles

Based on that vision, the development team for The Grove at Shoal Creek developed the following Guiding Principles for the project. These principals where considered in developing the Master Plan and this document.

2.2.1 Create an inviting and integrated project that enhances the experience of the site and its surroundings

2.2.2. Develop a comprehensive built environment with high-quality parks and open spaces shaped by massing and appropriately scaled to their context

2.2.3 Establish a vibrant, people-oriented development pattern that promotes connectivity and prioritizes pedestrian and bicycle circulation over cars

2.2.4 Establish a restorative approach to the development of the site by integrating sustainable strategies and honoring the history and natural character of the land

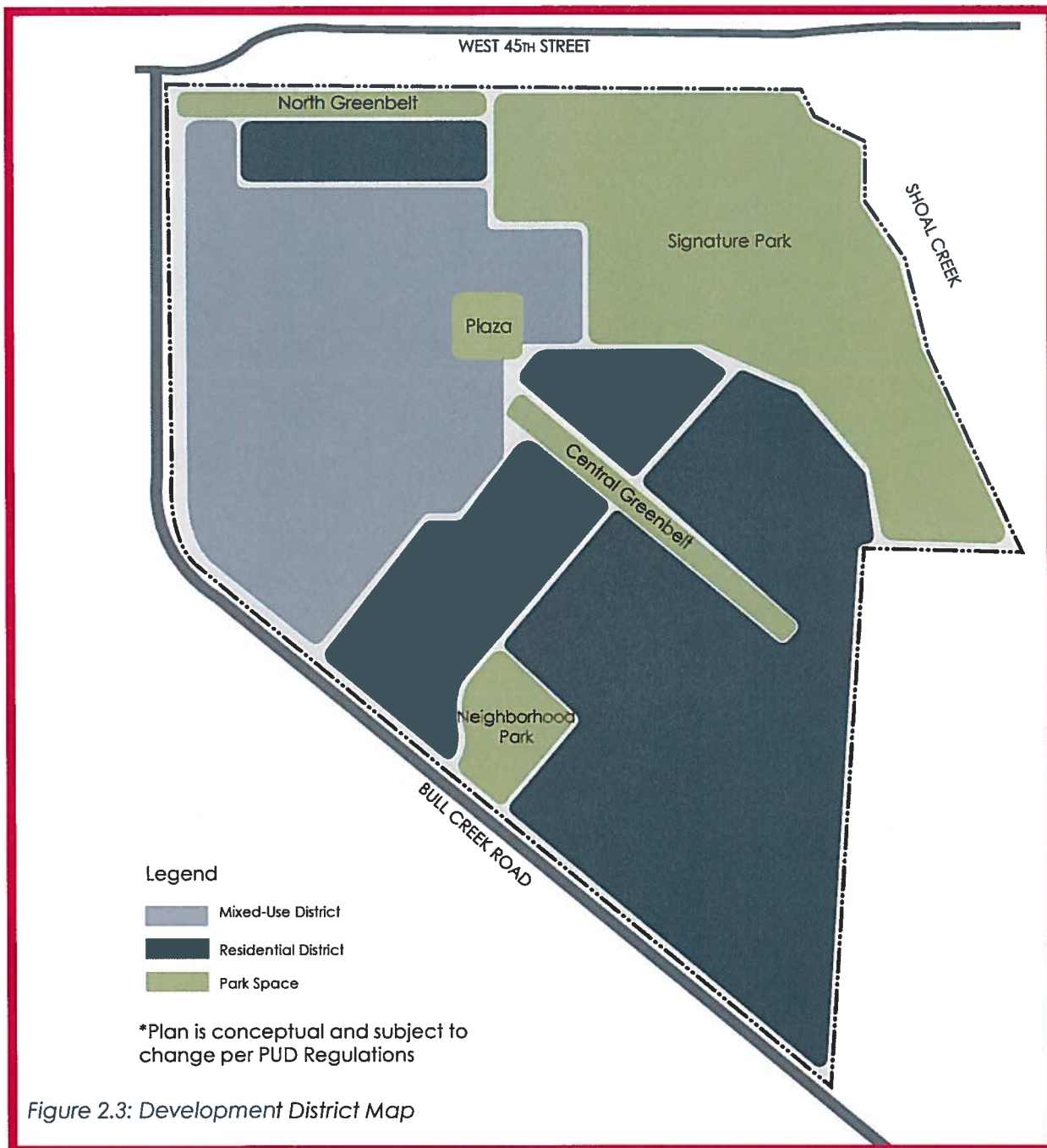
2.2.5. Create an economically viable development model that maximizes the mix of uses and captures the essence of Austin living.

2.2.6 Shift the conventional interaction between developer and neighborhood to a shared-purpose relationship

2.3 Development Districts

The project is composed of two development districts and a number of park and open space elements. The Development District Map below shows the conceptual layout of these districts on the site. For each building or project on the site, the applicant may select the most appropriate district for the desired use. As described in Section 4, the Architectural Design Guidelines for that building will be based on the selected district.

The Development District Map shown here is conceptual in nature and is not intended as a regulating document. Land Use regulations shall be governed by the approved Land Use Plan in the PUD zoning ordinance approved by the city of Austin on XXXX. Each building or project may select the most appropriate district for the desired project if the project is located in a Tract where that district is allowed, as described in Section 2.4.



2.4 Description of Development Districts



2.4.1 The Mixed-Use District. The heart of the master plan, this district contains a vibrant mix of uses which may include retail, office, high-density residential, live/work, and/or congregate care. This district is allowed in Tracts B, F, and G of the Land Use Plan.



2.4.2 The Residential District. This district contains a mix of for-sale and rental housing products including detached residential, townhomes, row houses, live-work units, stacked flats, and apartments. This district is allowed in all Tracts of the Land Use Plan.



2.4.3 Parks and Open Space. The parks and open space component of The Grove at Shoal Creek shall consist, at a minimum, of the Signature Park, Pocket Park, Central Greenbelt, North Greenbelt, and Plaza. Approximate locations and sizes are depicted in Figure 2.3. Park buildings and park structures are premitted in parks and open space.

2.5 Planned Unit Development (PUD) Land Use Plan

The Land Use Plan, shown here for reference, as adopted in the City's zoning ordinance, provides the land use regulations for the project and asserts the site development regulations for each tract, including height, FAR, setbacks, and impervious cover limitations for each parcel.

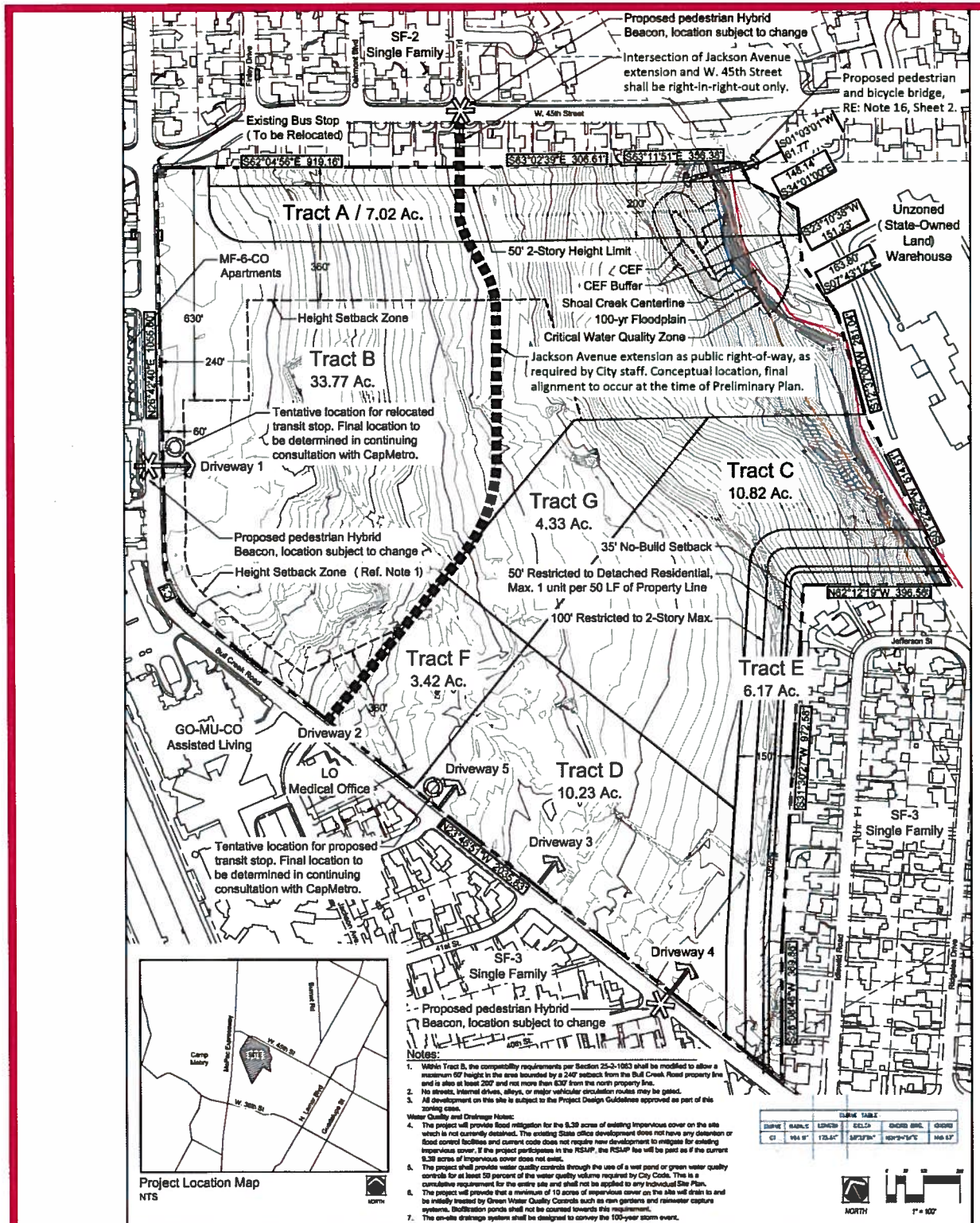
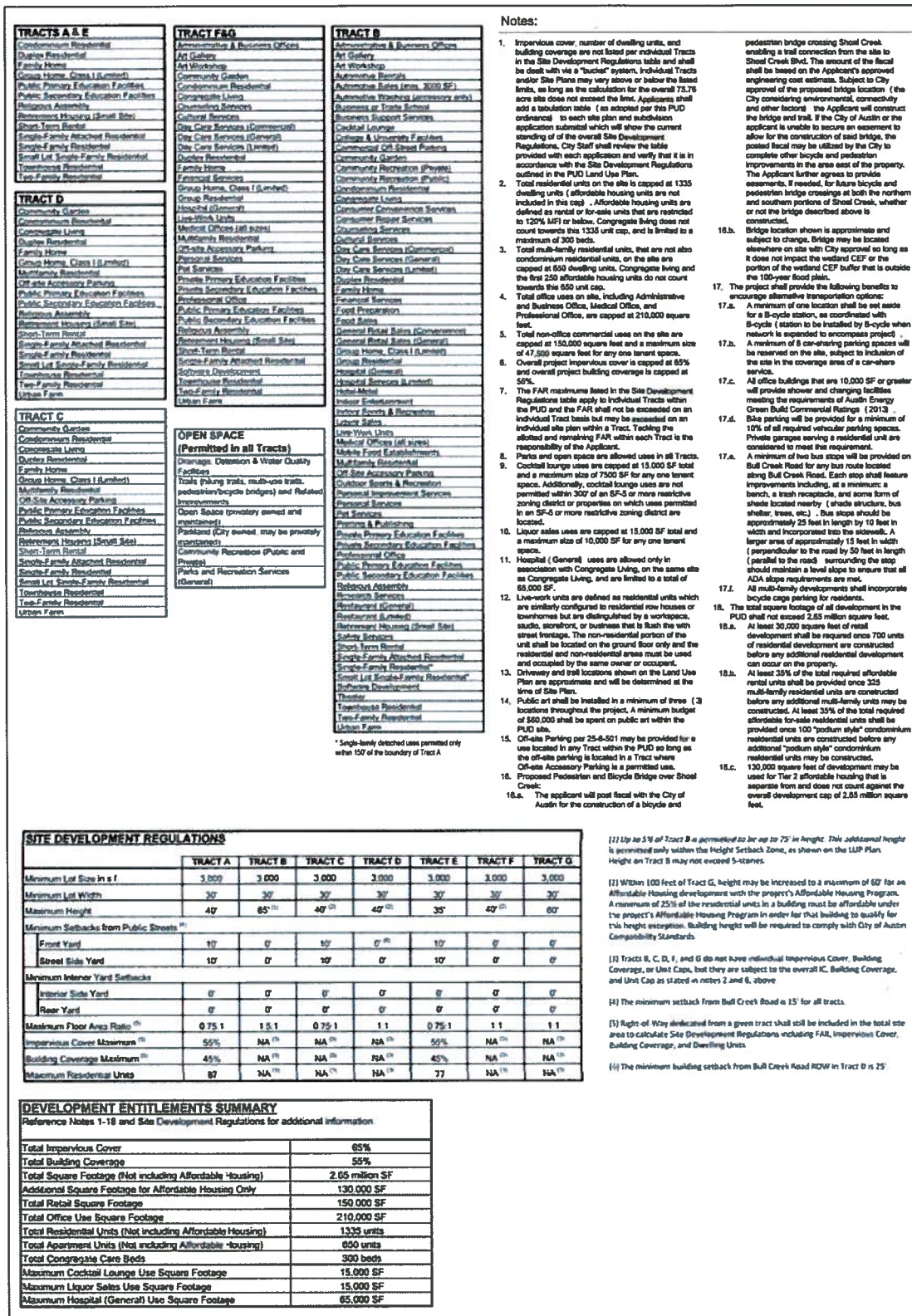


Figure 2.5: PUD Land Use Plan



SITE DEVELOPMENT REGULATIONS

	TRACT A	TRACT B	TRACT C	TRACT D	TRACT E	TRACT F	TRACT G
Minimum Lot Size in s.f.	3,000	3,000	3,000	3,000	3,000	3,000	3,000
Minimum Lot Width	30'	30'	30'	30'	30'	30'	30'
Maximum Height	40'	65' ⁽¹⁾	40' ⁽²⁾	40' ⁽²⁾	35'	50' ⁽³⁾	60'
Minimum Setbacks from Public Streets ⁽⁴⁾							
Front Yard	10'	0'	10'	0' ⁽⁵⁾	10'	0'	0'
Street Side Yard	10'	0'	10'	0'	10'	0'	0'
Minimum Interior Yard Setbacks							
Interior Side Yard	0'	0'	0'	0'	0'	0'	0'
Rear Yard	0'	0'	0'	0'	0'	0'	0'
Maximum Floor Area Ratio ⁽⁶⁾	0.75:1	1.5:1	0.75:1	1:1	0.75:1	1:1	1:1
Impervious Cover Maximum ⁽⁷⁾	55%	NA ⁽⁸⁾	NA ⁽⁸⁾	NA ⁽⁸⁾	55%	NA ⁽⁸⁾	NA ⁽⁸⁾
Building Coverage Maximum ⁽⁹⁾	45%	NA ⁽⁸⁾	NA ⁽⁸⁾	NA ⁽⁸⁾	45%	NA ⁽⁸⁾	NA ⁽⁸⁾
Maximum Residential Units	87	NA ⁽¹⁰⁾	NA ⁽¹⁰⁾	NA ⁽¹⁰⁾	77	NA ⁽¹⁰⁾	NA ⁽¹⁰⁾

DEVELOPMENT ENTITLEMENTS SUMMARY

Reference Notes 1-10 and Site Development Regulations for additional information.

Total Impervious Cover	65%
Total Building Coverage	55%
Total Square Footage (Not including Affordable Housing)	2.05 million SF
Additional Square Footage for Affordable Housing Only	130,000 SF
Total Retail Square Footage	150,000 SF
Total Office Use Square Footage	210,000 SF
Total Residential Units (Not including Affordable Housing)	1335 units
Total Apartment Units (Not including Affordable Housing)	650 units
Total Congregate Care Beds	300 beds
Maximum Cocktail Lounge Use Square Footage	15,000 SF
Maximum Liquor Sales Use Square Footage	15,000 SF
Maximum Hospital (General) Use Square Footage	65,000 SF

Figure 2.5.1: PUD Land Use Regulations

3.0 FRAMEWORK

3.1 Intent

Section 3 of The Grove at Shoal Creek Design Guidelines establishes standards and requirements for the framework of streets, sidewalks, paths, trails, alleys, and other circulation routes. These Framework Elements will organize the site and define not only the site's transportation system, but also some of its most important public spaces and pedestrian environments. The intent of this Section is to:



3.1.1 Create a basic framework for organizing the site and ensuring that buildings and other elements can relate appropriately to the street to create a cohesive visual identity and attractive street scene;



3.1.2 Ensure efficient pedestrian, bicycle, and vehicular circulation that is people-oriented, promotes connectivity, and prioritizes pedestrian and bicycles circulation over cars;

3.1.3 Create a high-quality street environment with street trees and properly scaled roadways and sidewalks;

3.1.4 Add urbanity to the street by providing opportunities for pedestrian friendly uses within and adjacent to the street;

3.1.5 Ensure that adequate vehicular parking is accommodated and well integrated into the street environment;



3.1.6 Provide opportunities for the integration of green infrastructure into the street environment;

3.1.7 Ensure that The Grove at Shoal Creek is developed as a comprehensive built environment.



3.2 Using This Section

This Section provides a Master Framework Plan indicating the overall layout of streets and other circulation patterns on the site. Each street, alley, or trail indicated on the Master Framework Plan is keyed to a specific cross section defining its dimensions, characteristics, and features.

Jackson Avenue will be a publicly dedicated street within The Grove at Shoal Creek. All other streets in The Grove at Shoal Creek will be privately owned and maintained but permanently accessible to the public. This means there is no public right-of-way, except for Bull Creek Road and Jackson Avenue. Instead, this document defines a "Street Zone" for each street, as well as supplemental "Greenway Zones" in certain instances. These zones establish the area in which the Framework Section sets the design standards. Other Sections of this document will set the Design Standards for spaces and elements outside the Street Zone, and may set standards for how those elements shall relate to the street using the Street Zone as a boundary line.

For the Retail Main Street, Green Streets, Secondary Retail Streets, and Residential Streets, the following additional standards apply:

1. Public access and utility easements (where needed) shall be provided for the entirety of the private street lengths, granting control to the City of Austin of all traffic elements for intersections between public right-of-way and any private streets/driveways within the development.
2. These streets shall be designed to include 50 feet minimum tangent for intersection approaches and a 100 feet minimum centerline radius for horizontal curves. Horizontal geometry for these streets may be varied with approval of the Director.

Dimensions are provided in the roadway sections that follow. These dimensions are labeled as follows:

- Min: Represents the minimum allowable dimension for this feature or space
- Max: Represents the maximum allowable dimension for this feature or space
- Approx: This dimension is approximate and may be modified as needed by the design team

Tree spacing is also provided within the roadway sections. In all areas, tree spacings are meant to represent an average spacing, and this average applies only to the length of the street between intersections. Tree spacing may be regular or irregular as appropriate to the individual design of the street and the limitations of utility locations, driveway locations, existing trees, and other existing or planned obstacles that may interfere with tree placement. Street trees are generally located in a planting zone that is a minimum of 7' wide. The planting zone shall be continuous and located adjacent to the curb. Utility compatible trees may be substituted for shade trees where utility conflicts exist. In some cases, trees may be provided adjacent to the Street Zone where utility and/or driveway conflicts prevent the placement of the tree within the street zone.

Rain gardens and biofiltration facilities are also shown in many of these sections. The feasibility of these features is subject to a number of engineering factors outside of the scope of these Design Guidelines including slope, utility conflicts, etc. While the PUD ordinance requires a certain amount of these facilities, and these facilities are generally allowable as shown and desired where feasible, they are not required in any given Street Zone or street section. Rather, the commitments made in the zoning ordinance to provide a certain percentage of the site's water quality through innovative water quality controls and to drain a certain percentage of the site's streets directly into rain gardens or other landscape features will dictate the minimum requirements for these features.

3.3 Master Framework Plan

The Master Framework Plan provides an overview of the possible layout of streets and other framework elements. Certain elements of the Master Framework Plan are considered Primary Framework Elements. The general location and orientation of these Primary Framework Elements should be as shown, only minor variations should be made as appropriate to improve alignments or traffic performance and optimize building parcels. By contrast final alignment and orientation of the Secondary Framework Elements is flexible and may vary from the Master Framework Plan so long as the final arrangement still creates a well-connected framework consistent with the Intent of this Section.

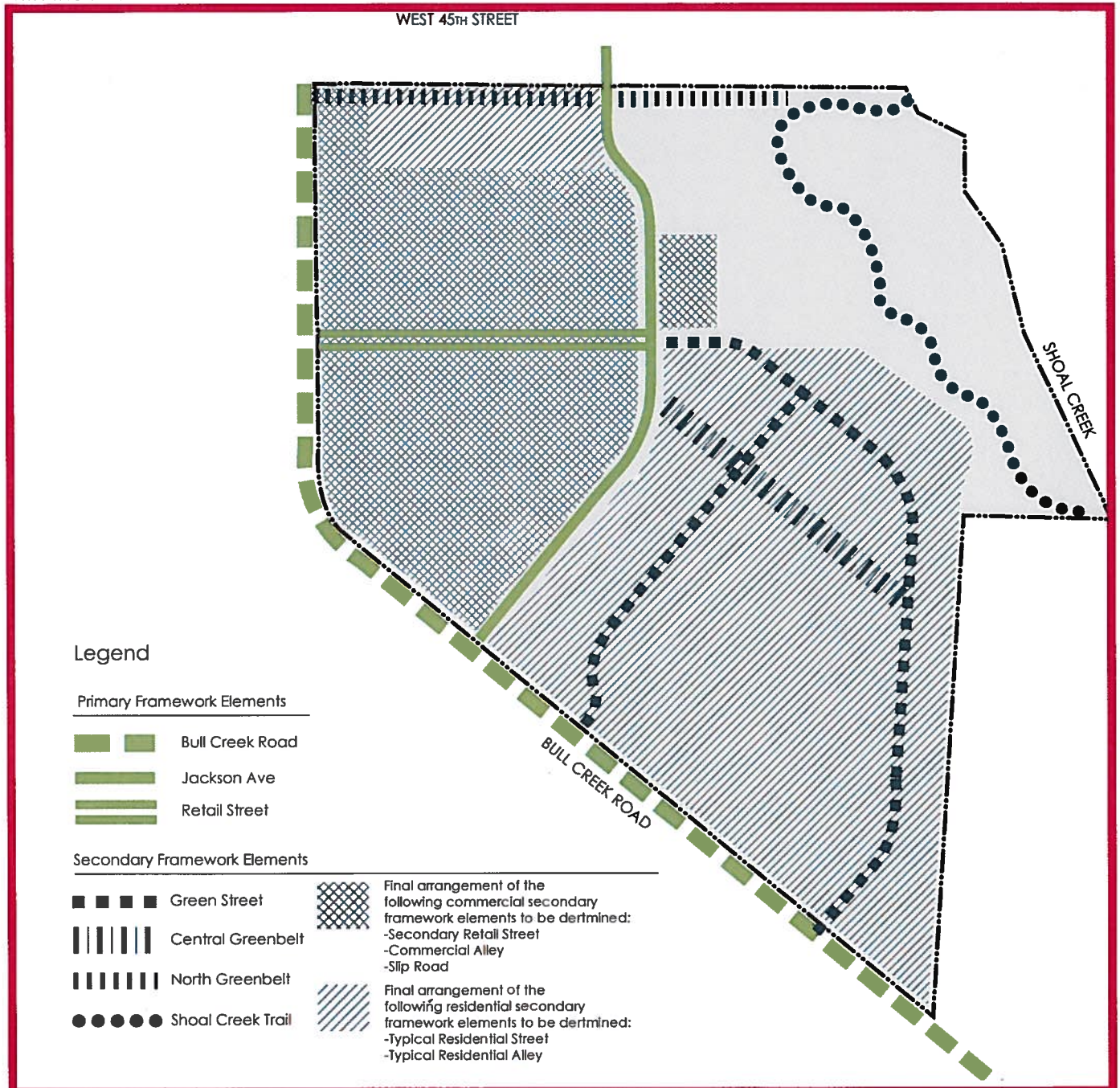


Figure 3.3: Master Framework Plan

3.4 Primary Framework Elements

3.4.1 Bull Creek Road

The intent of Bull Creek Road is to create a high-quality edge for the project that is inviting and provides exceptional facilities for pedestrians and bicyclists. These standards define the relationship of the site and the Bull Creek Road Trail to Bull Creek Road. The final design of the roadway within the right-of-way will be determined by the project's Traffic Engineer and is not governed by this document.

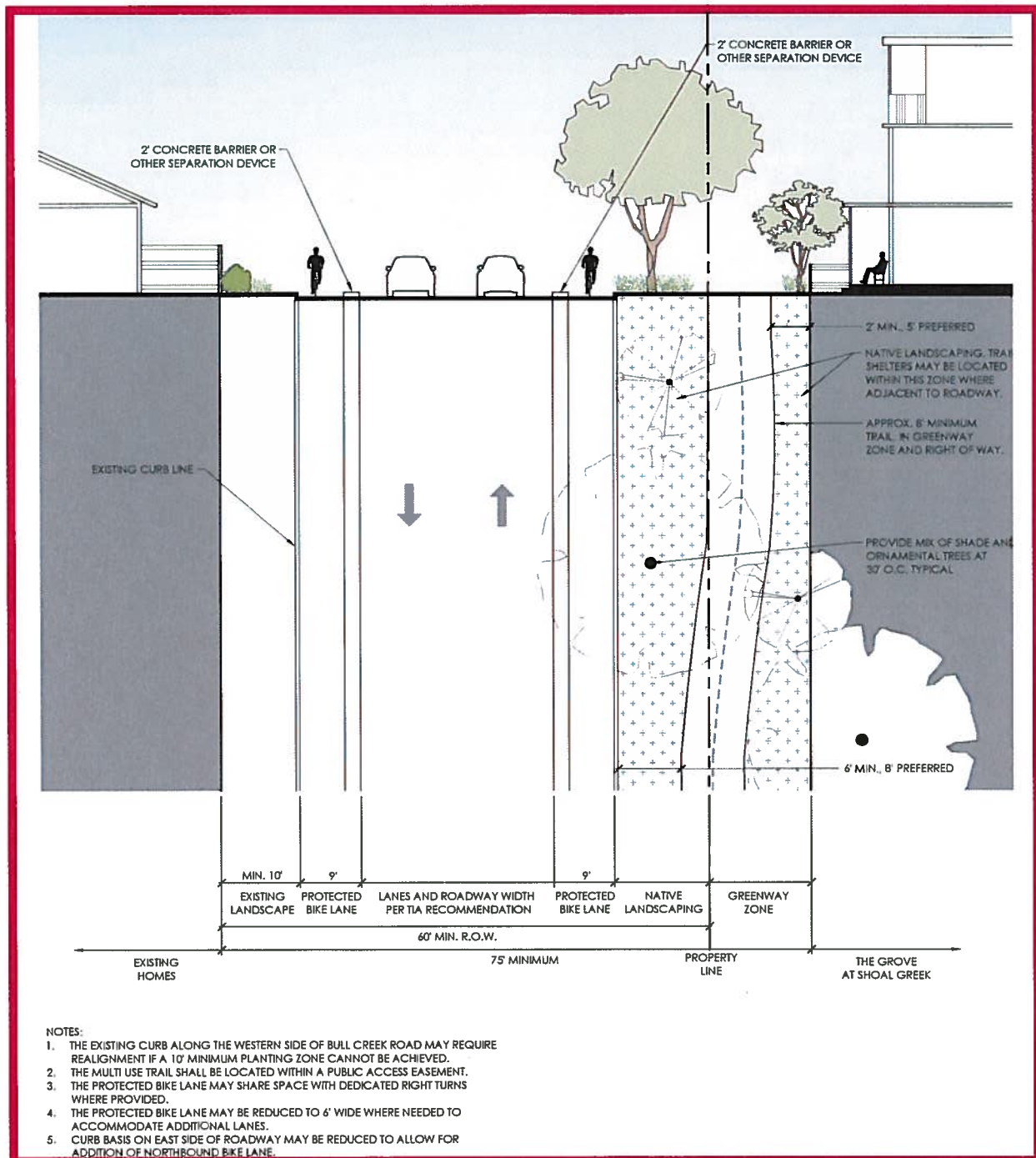


Figure 3.4.1: Bull Creek Road

3.4.2 Retail Main Street

The intent of the Retail Main Street is to create a wide, comfortable pedestrian environment that is conducive to successful retail uses and promotes interaction between users.

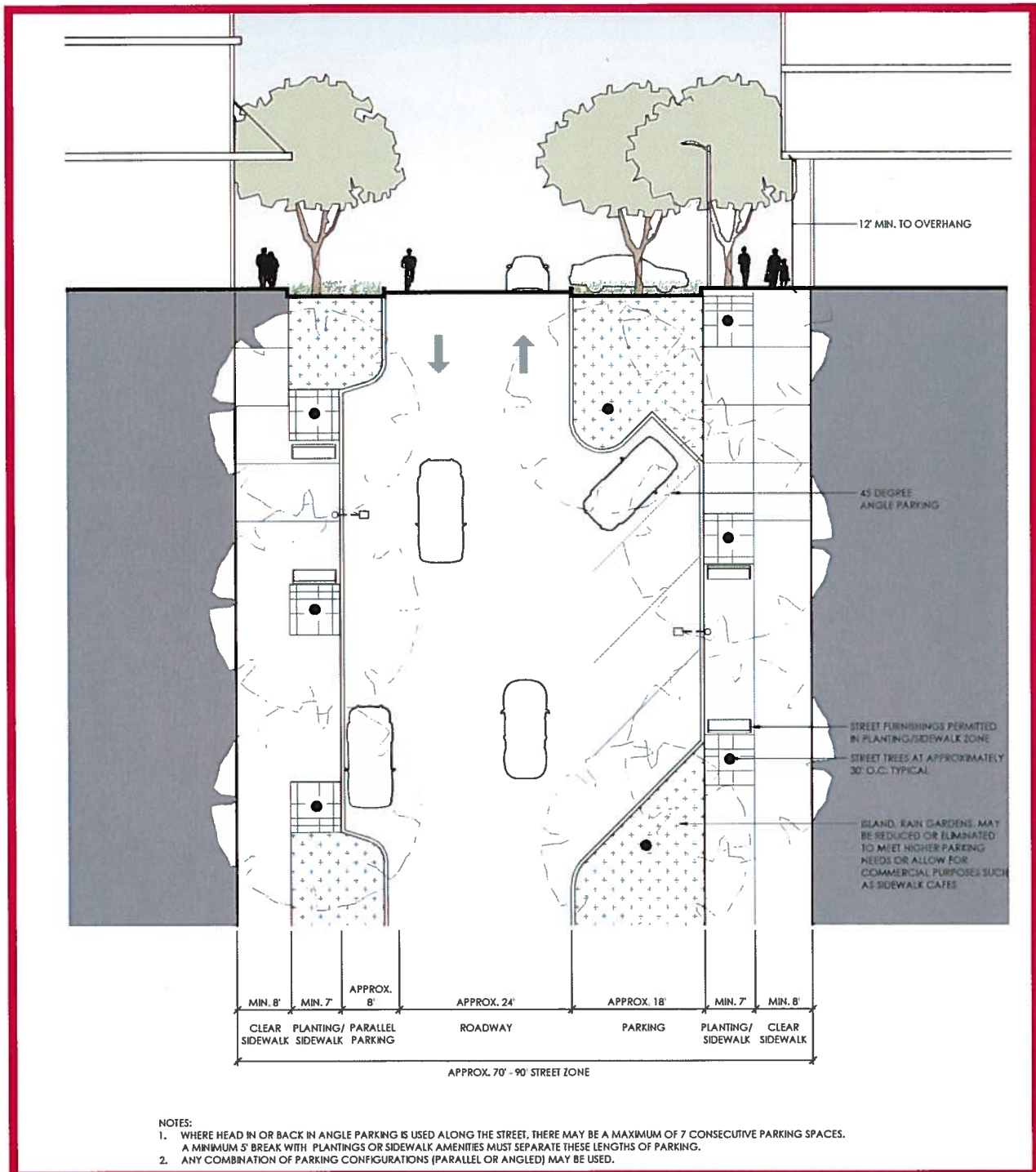


Figure 3.4.2.a: Retail Main Street

3.4.2 Retail Main Street - Parkway Alternative

This section is allowable in lieu of Section 3.4.2a for any segment of the Retail Main Street to provide for a parkway space in the middle of the roadway. This section will add open space and vibrancy to key segments of the Retail Main Street. Angle parking configurations are also permitted with this street section.

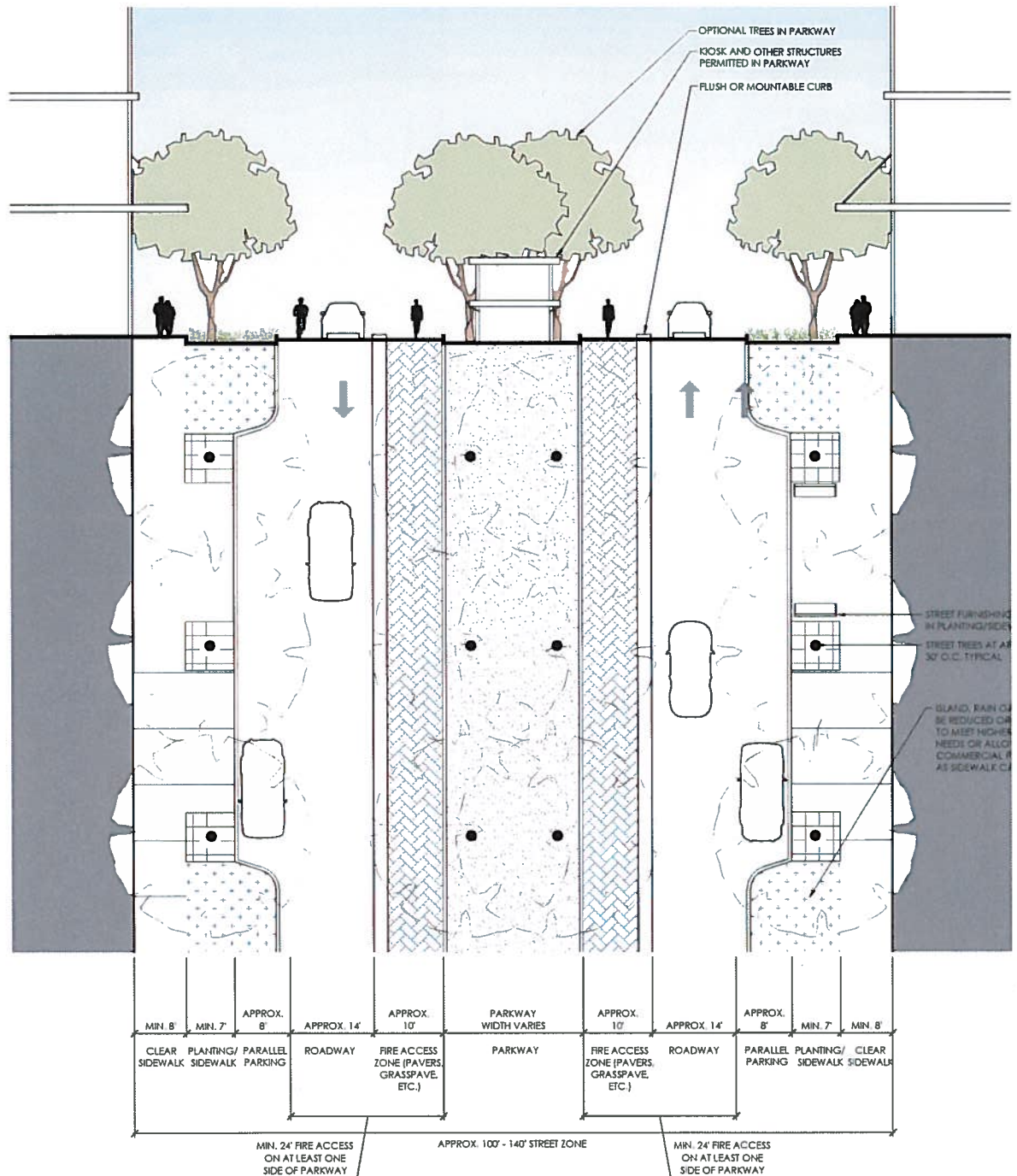


Figure 3.4.2.b: Retail Main Street - Parkway Alternative

3.4.3 Jackson Avenue Extension

The Jackson Avenue Extension is the site's primary collector road. The intent is to accommodate a larger volume of vehicular traffic than the site's other roadways while still promoting a high quality pedestrian environment. Two potential sections are provided to allow for options with how this street deals with bicycles. A minimum of

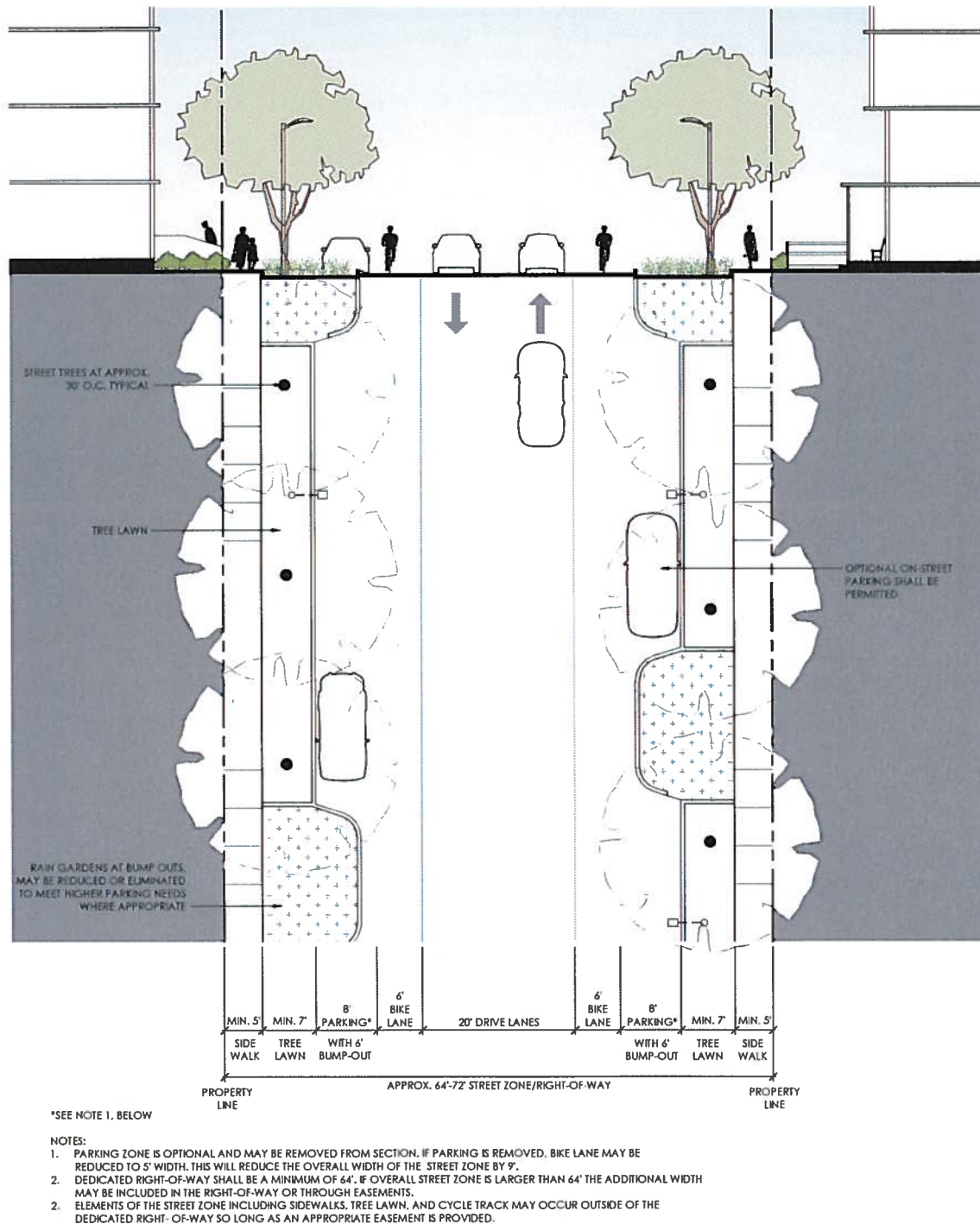
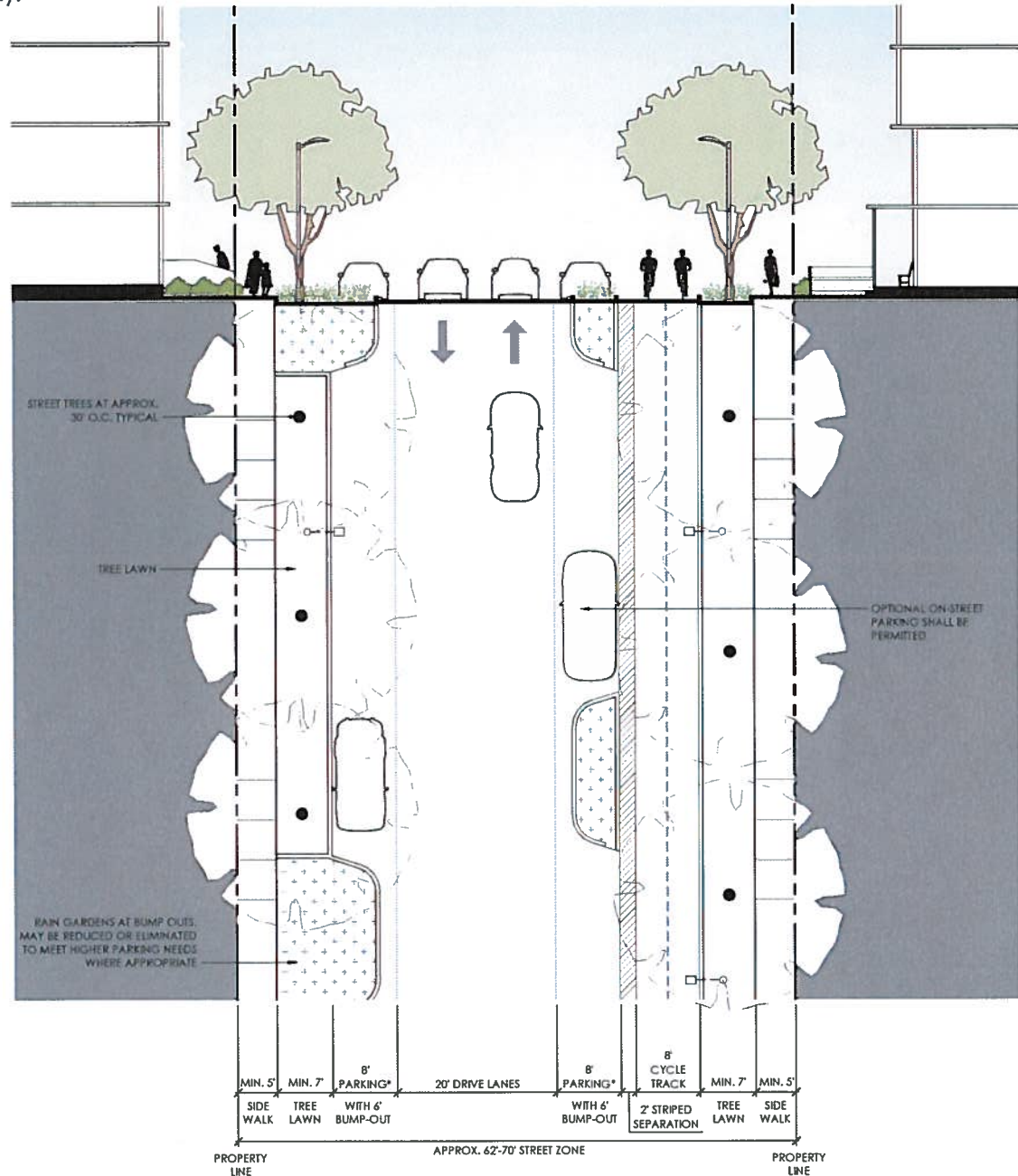


Figure 3.4.3.a: Jackson Avenue Extension

62' of right-of-way shall be dedicated for Jackson Avenue. If the total Roadway Zone exceeds 62' of width, the applicant may choose to dedicate additional right-of-way or to dedicate public access easements for the remainder of the street zone. All roadway paving and bike lane / cycle track paving must be contained within the right-of-way.



*SEE NOTE 1, BELOW

NOTES:

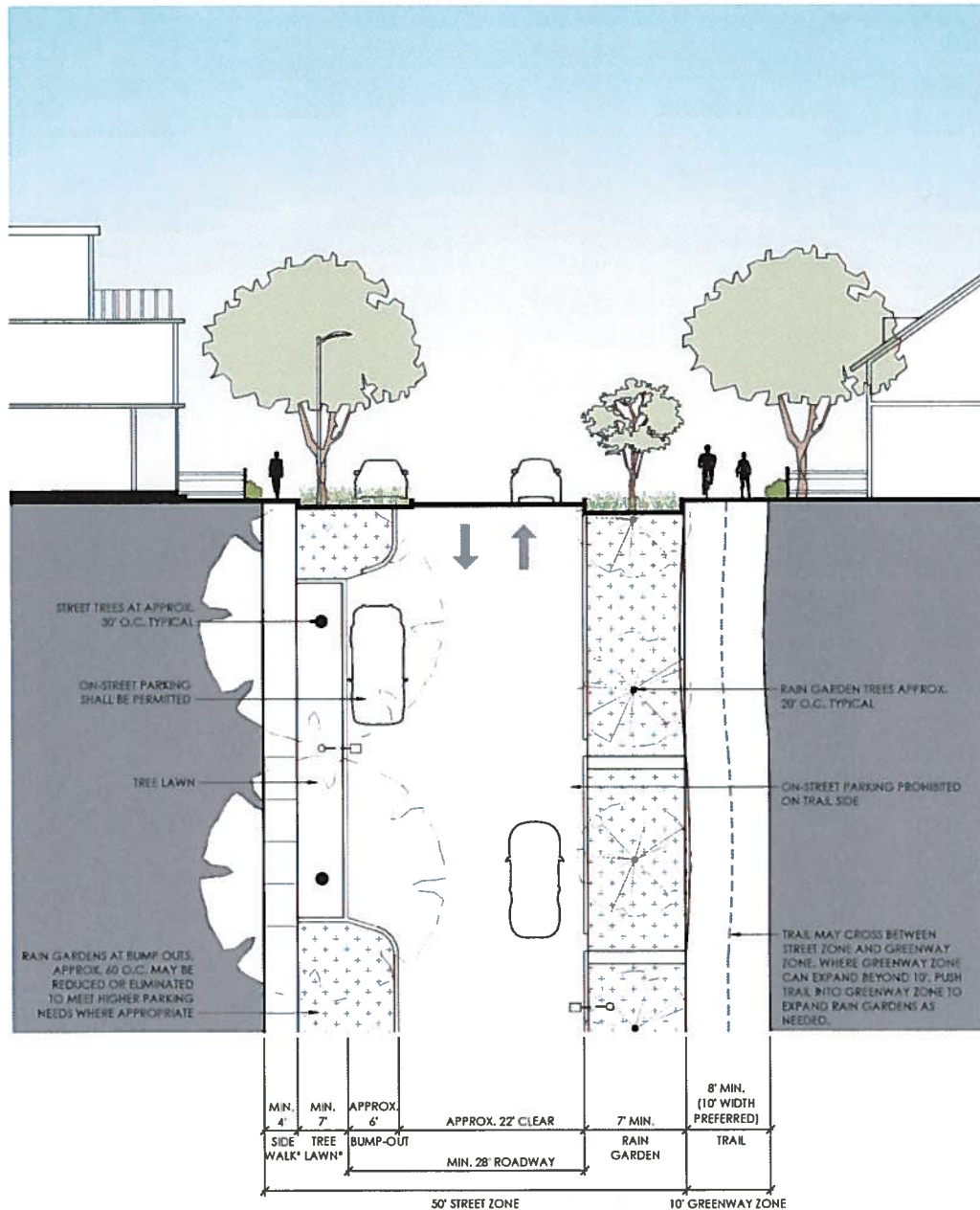
1. PARKING ZONE IS OPTIONAL AND MAY BE REMOVED FROM SECTION. IF PARKING IS REMOVED, BIKE LANE MAY BE REDUCED TO 5' WIDTH. THIS WILL REDUCE THE OVERALL WIDTH OF THE STREET ZONE BY 9'.
2. DEDICATED RIGHT-OF-WAY SHALL BE A MINIMUM OF 62'. IF OVERALL STREET ZONE IS LARGER THAN 62' THE ADDITIONAL WIDTH MAY BE INCLUDED IN THE RIGHT-OF-WAY OR THROUGH EASEMENTS.
2. ELEMENTS OF THE STREET ZONE INCLUDING SIDEWALKS, TREE LAWN, AND CYCLE TRACK MAY OCCUR OUTSIDE OF THE DEDICATED RIGHT-OF-WAY SO LONG AS AN APPROPRIATE EASEMENT IS PROVIDED.

Figure 3.4.3.b: Jackson Avenue Extension

3.5 Secondary Framework Elements

3.5.1 Green Streets

The sites Green Streets are designed to accommodate safe movement through the site and to the park for pedestrians and cyclists in particular. They also form a key element in the site's green infrastructure system.



NOTES:

1. A 5' MINIMUM SIDEWALK AND A 7' MINIMUM TREE LAWN ARE REQUIRED IF THE ROADWAY ABUTS A NON-RESIDENTIAL USE.
2. WHERE THE ROADWAY ABUTS A PARK, THE REQUIRED SIDEWALK OR TRAIL MAY BE LOCATED WITHIN THE PARK SO LONG AS IT IS LOCATED WITHIN 15' OF THE ROADWAY.
3. WHERE THE ROADWAY ABUTS A PARK, HEAD-IN OR ANGLED PARKING MAY REPLACE PARALLEL PARKING. IN THESE CASES, THE STREET ZONE WILL NEED TO EXPAND AS NECESSARY TO ACCOMMODATE THIS PARKING LAYOUT.

Figure 3.5.1: Green Streets

3.5.2 Central Greenbelt

The Central Greenbelt links the pedestrian and mixed-use zones of the site, and is designed to promote casual interaction between residents and visitors to the site.

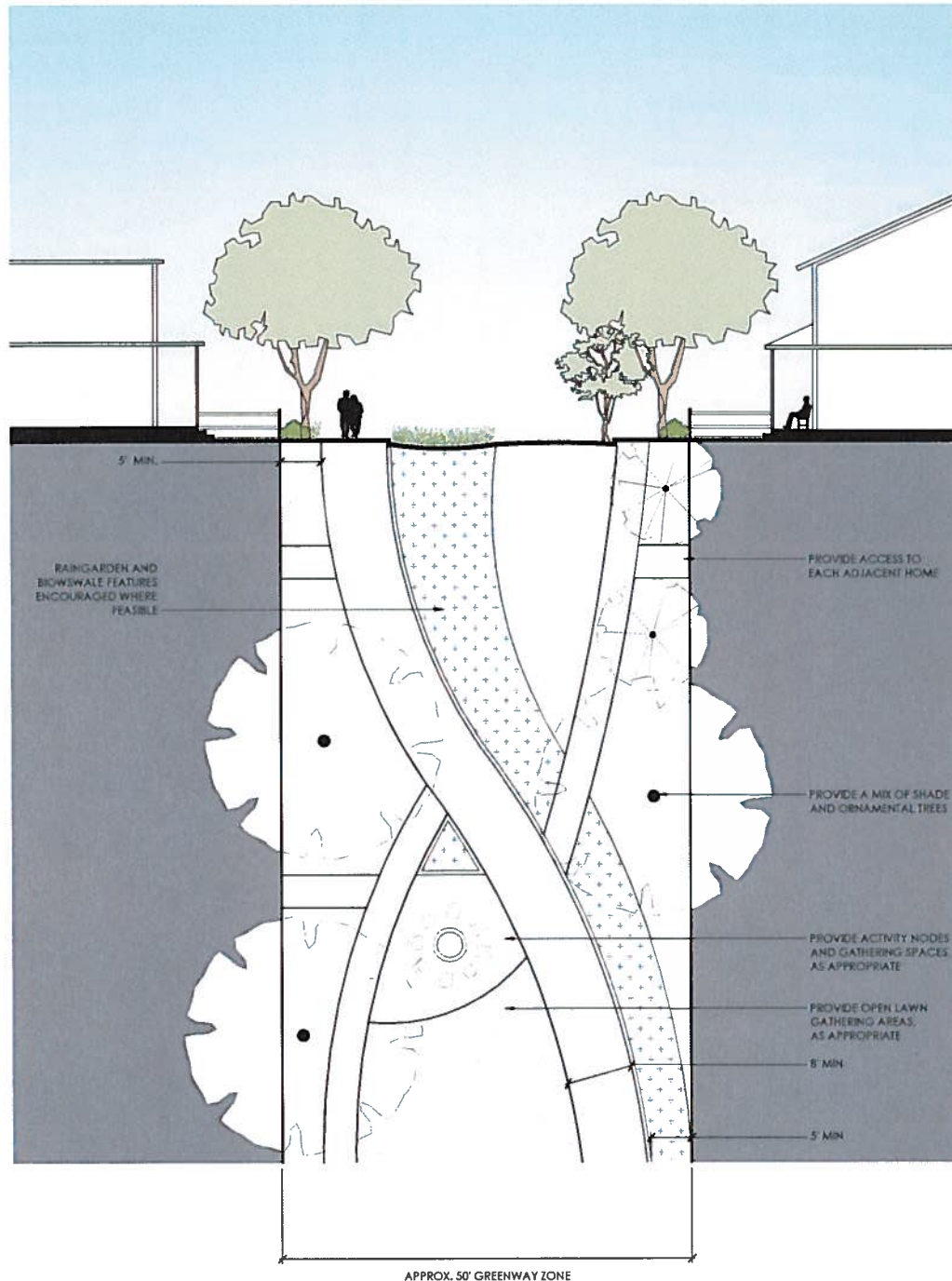
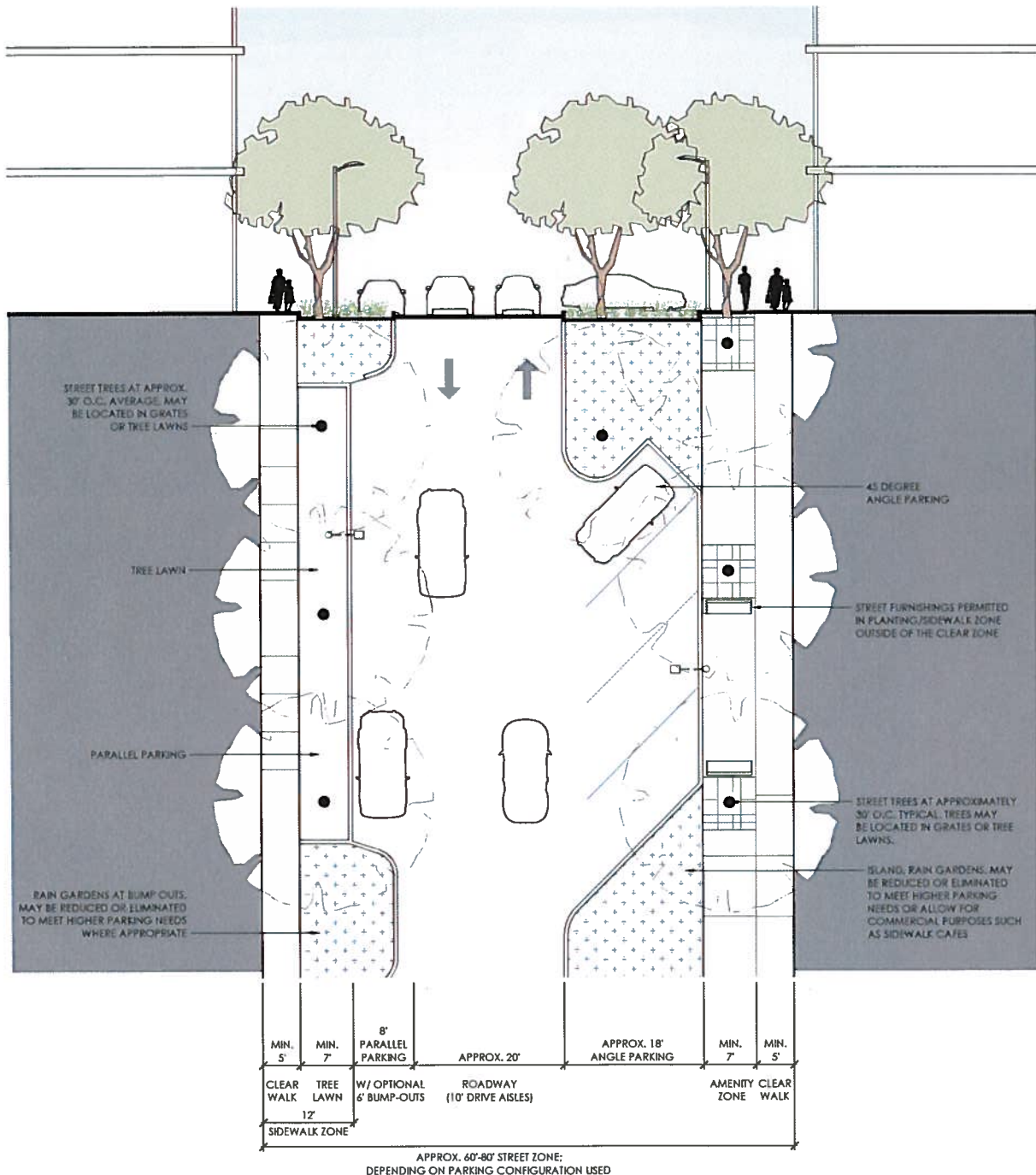


Figure 3.5.2: Central Greenbelt

3.5.3 Secondary Retail Street

The Secondary Retail Street is designed to accommodate a higher volume of parking within the Street Zone while still maintaining a street-like character. Either head-in angled or parallel parking options may be used on either side of the street.



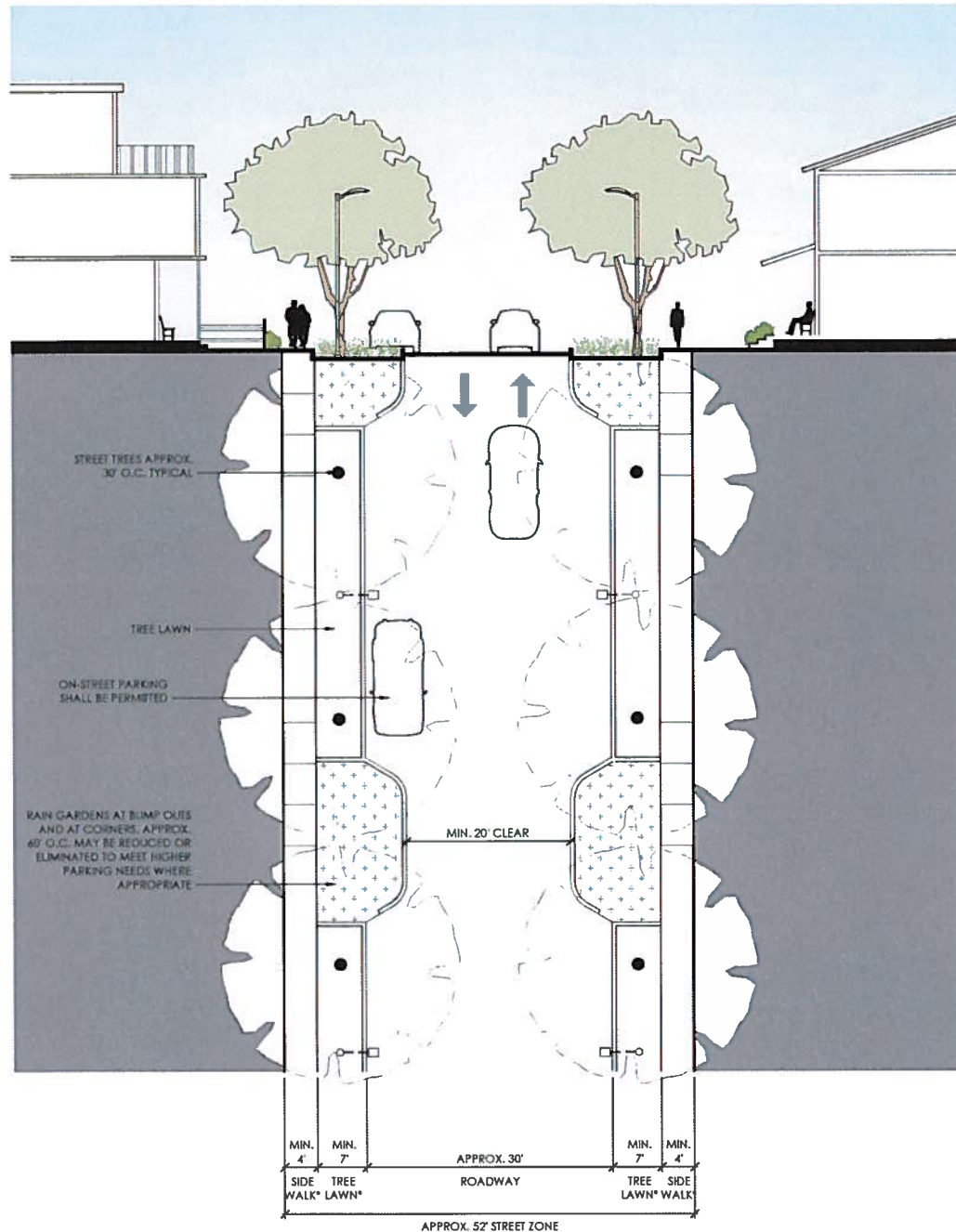
NOTES:

1. ANY COMBINATION OF THE TWO PARKING CONFIGURATIONS (PARALLEL OR ANGLED) AND TWO SIDEWALK CONFIGURATIONS (TREE LAWN OR TREE GRATES) SHOWN ABOVE MAY BE USED.

Figure 3.5.3: Secondary Retail Street

3.5.4 Residential Streets

The residential streets are designed to create a high quality residential street that will moderate traffic speeds, allow for convenient guest parking, and integrate street trees and green infrastructure into the streetscape.



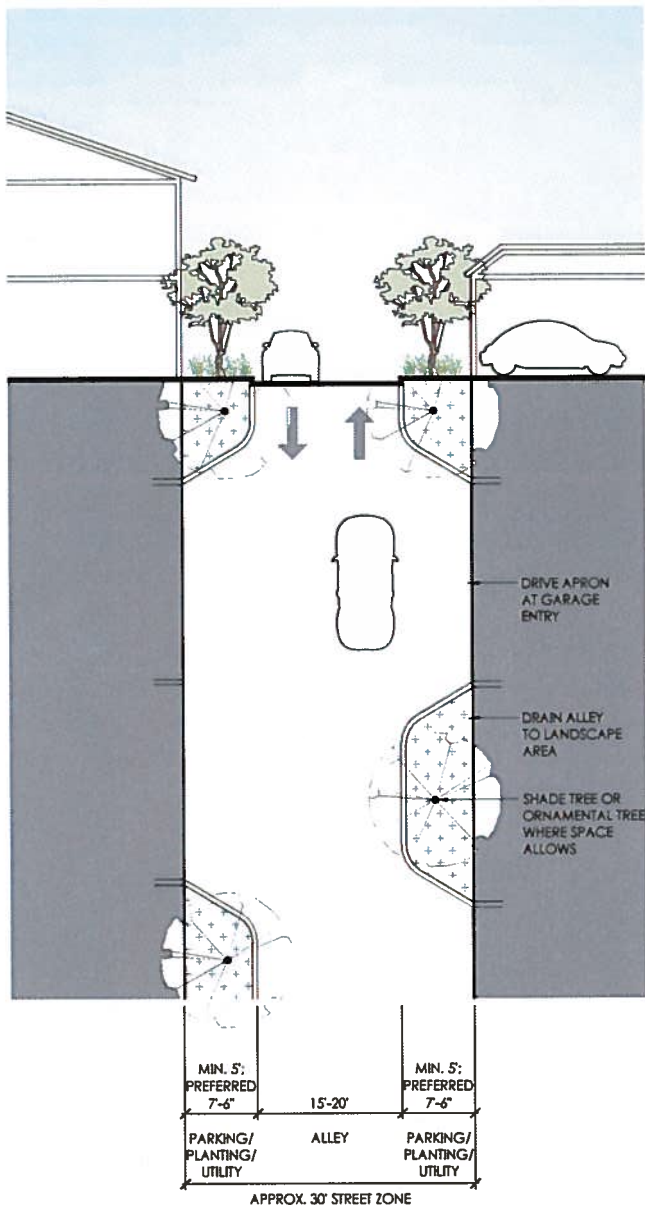
NOTES:

1. A 5' MINIMUM SIDEWALK AND A 7' MINIMUM TREE LAWN ARE REQUIRED IF THE ROADWAY ABUTS A NON-RESIDENTIAL USE.
2. WHERE THE ROADWAY ABUTS A PARK, THE REQUIRED SIDEWALK OR TRAIL MAY BE LOCATED WITHIN THE PARK SO LONG AS IT IS LOCATED WITHIN 15' OF THE ROADWAY.
3. WHERE THE ROADWAY ABUTS A PARK, HEAD-IN OR ANGLED PARKING MAY REPLACE PARALLEL PARKING. IN THESE CASES, THE STREET ZONE WILL NEED TO EXPAND AS NECESSARY TO ACCOMMODATE THIS PARKING LAYOUT.
4. THERE SHALL BE A MAXIMUM OF 200' BETWEEN BUMP-OUTS WHERE THERE IS ON STREET PARKING.

Figure 3.5.4: Residential Streets

3.5.5 Typical Residential Alley

The intent of the typical alley is to provide a functional alley that maximizes the visual appeal and integrates as much landscaping as possible. The smaller pavement section should be used wherever feasible and expanded where necessary for fire access.



NOTES:

1. ALLEY WIDTH SHALL BE 20' WHERE FIRE ACCESS IS REQUIRED.
2. ALLEY WIDTHS LESS THAN 16' ARE INTENDED FOR ONE-WAY TRAFFIC. DIRECTIONAL SIGNAGE IS REQUIRED AT ALLEY ACCESS POINTS FOR ONE-WAY ALLEYS.

3.5.6 Commercial Alley

Commercial Alleys are allowable at The Grove at Shoal Creek and are generally encouraged where they would minimize the conflicts and visual impacts created by the service functions and utility requirements of retail and other commercial buildings. A specific section is not provided for Commercial Alleys, but they should generally be designed similar to the residential alleys to include as much landscaping as feasible.

Figure 3.5.5: Typical Residential Alley

3.5.7 North Greenbelt Trail

The North Greenbelt trail will provide convenient access to the Signature Park and function as an east/west pedestrian and bicycle path on the south side of 45th Street (south of the existing homes). An optional 12' wide alley may be provided along the north property line to provide access to the existing homes which front on 45th Street at the developer's discretion and may be constructed only if allowed by the City of Austin. If the alley is constructed additional building setback from the north property line may be required to ensure the greenway zone still meets the minimum acreage designated in the Parks Plan for the North Greenbelt.

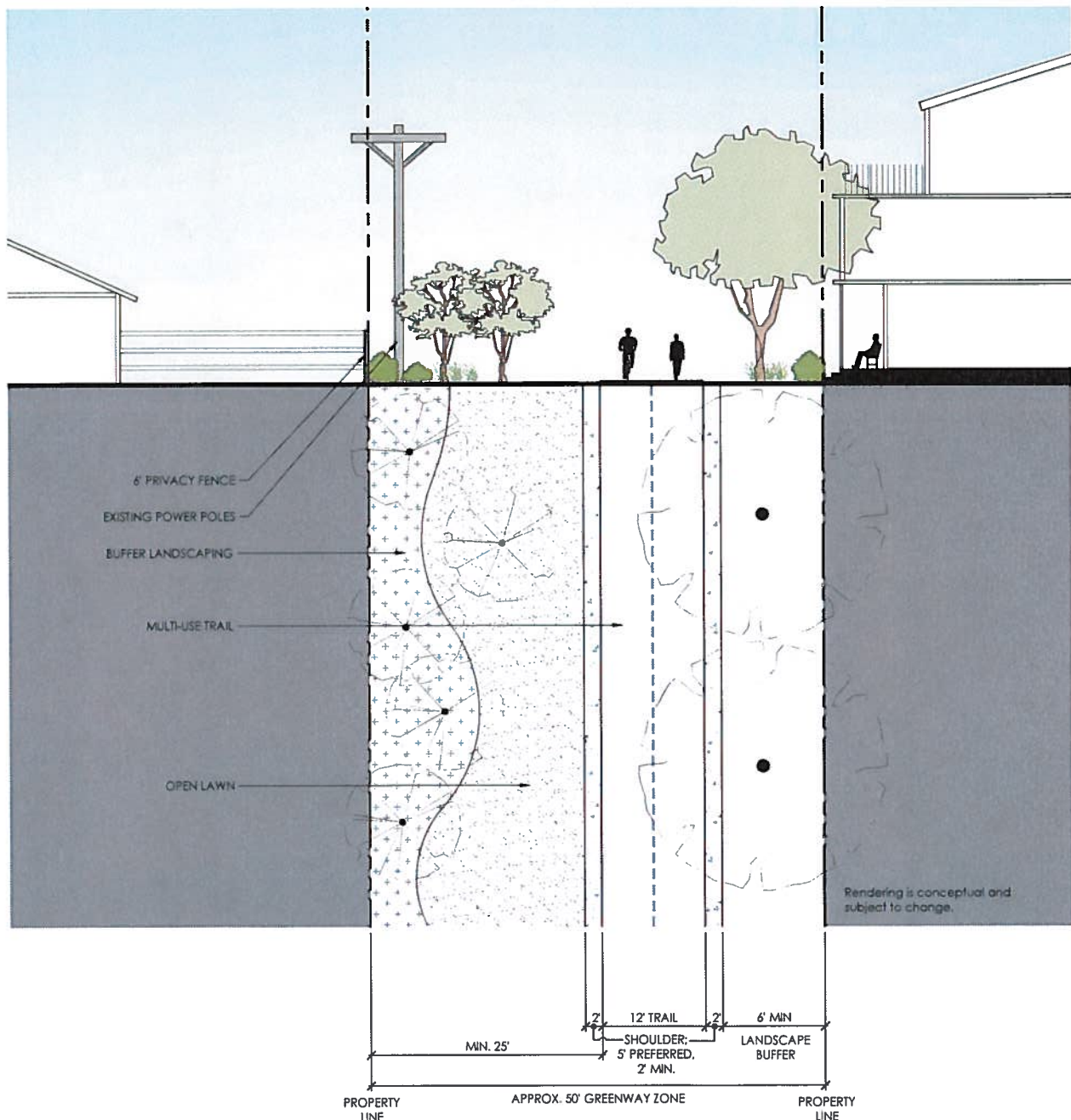
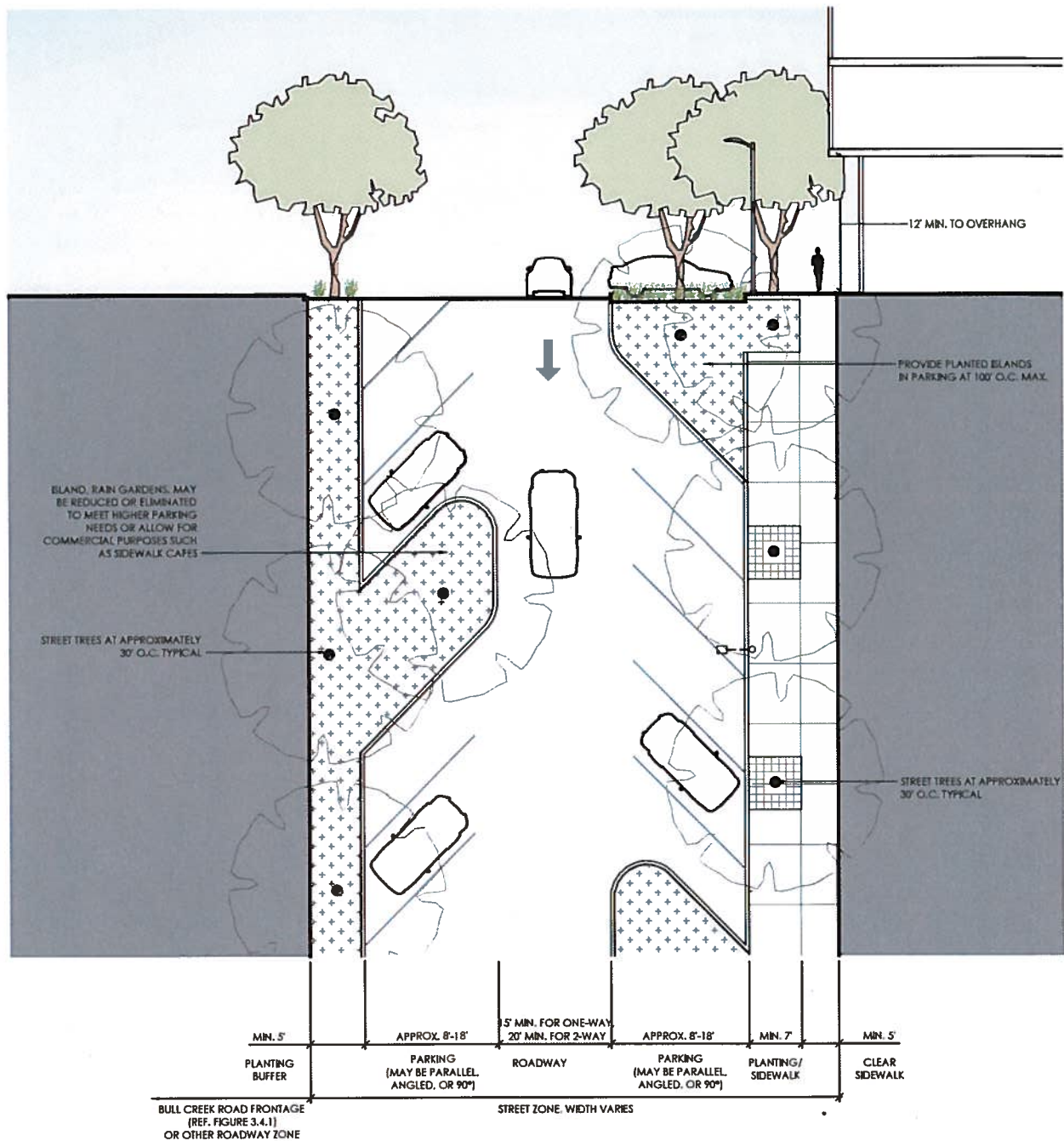


Figure 3.5.7: North Greenbelt Trail

3.5.8 Slip Road

The intent of the slip road is to provide safe access for on-street parking parallel to Bull Creek Road without obstructing traffic flow on Bull Creek Road. This framework element may be used between development parcels and Bull Creek Road and elsewhere on the site if appropriate.



NOTES:

1. WHERE HEAD IN OR BACK IN ANGLE PARKING IS USED ALONG THE STREET, THERE MAY BE A MAXIMUM OF 7 CONSECUTIVE PARKING SPACES. A MINIMUM 5' BREAK WITH PLANTINGS OR SIDEWALK AMENITIES MUST SEPARATE THESE LENGTHS OF PARKING.
2. ANY COMBINATION OF PARKING CONFIGURATIONS (PARALLEL OR ANGLED) MAY BE USED.

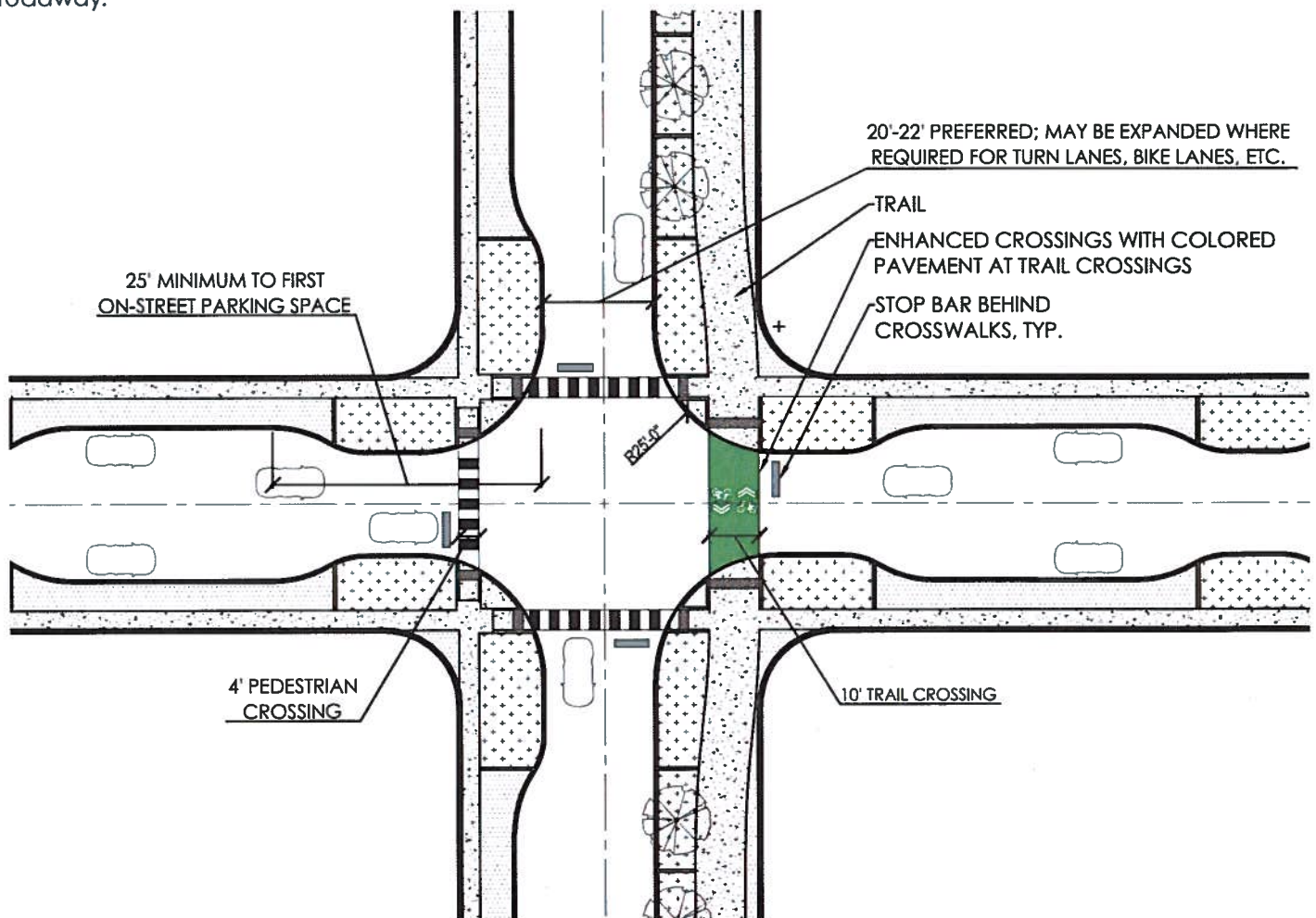
Figure 3.5.8: Slip Road

3.6 Typical Intersection Design

Intersections will take many distinct forms throughout The Grove at Shoal Creek as different street types intersect, additional turn lanes may occasionally be appropriate, some skew may be required, and bike lanes, trails, traffic controls, and other elements all impact the final design of an intersection. The typical intersections shown here are intended to set a general standard for intersections within The Grove at Shoal Creek that move traffic calmly but efficiently, provide for safe interactions between various modes of transportation, and contribute to the overall creation of a high quality, safe, and walkable urban environment.

3.6.1 Typical Intersection

The intersection shown here is between a Green Street and a typical residential street, but it reflects many of the qualities desired for all of the intersections at The Grove including minimal turning radii, bump-outs to shorten pedestrian crossings, clearly marked crosswalks, and clean integration of landscape, sidewalk, and roadway.



NOTES:

1. REFERENCE STREET STANDARDS FOR REQUIREMENTS FOR ROADWAYS, SIDEWALKS, STREET TREES, ETC.
2. ACCESSIBLE CURB RAMPS SHALL BE PROVIDED FOR ALL SIDEWALKS AT INTERSECTIONS UNLESS AN ACCESSIBLE ROUTE IS NOT POSSIBLE DUE TO SITE CONSTRAINTS. AN ACCESSIBLE ROUTE IS REQUIRED ON AT LEAST ONE SIDE OF ALL STREETS.

Figure 3.6.1: Typical Intersection Layout

3.6.2 Typical Traffic Circle

Roundabouts in the project are envisioned to help distribute traffic while also performing an important aesthetic function. The design below is conceptual and intended to communicate design intent, rather than to lock in specific dimensions, and may be modified based on final street design, etc. Because this facility is designed for relatively low vehicular speeds, the safest solution for cyclists is to merge with the vehicular lane and traverse the roundabout in the same manner as a vehicle. Cyclists who chose may dismount at the pedestrian ramp and instead traverse the roundabout as a pedestrian. This is an optional facility and may not occur on the final site plan.

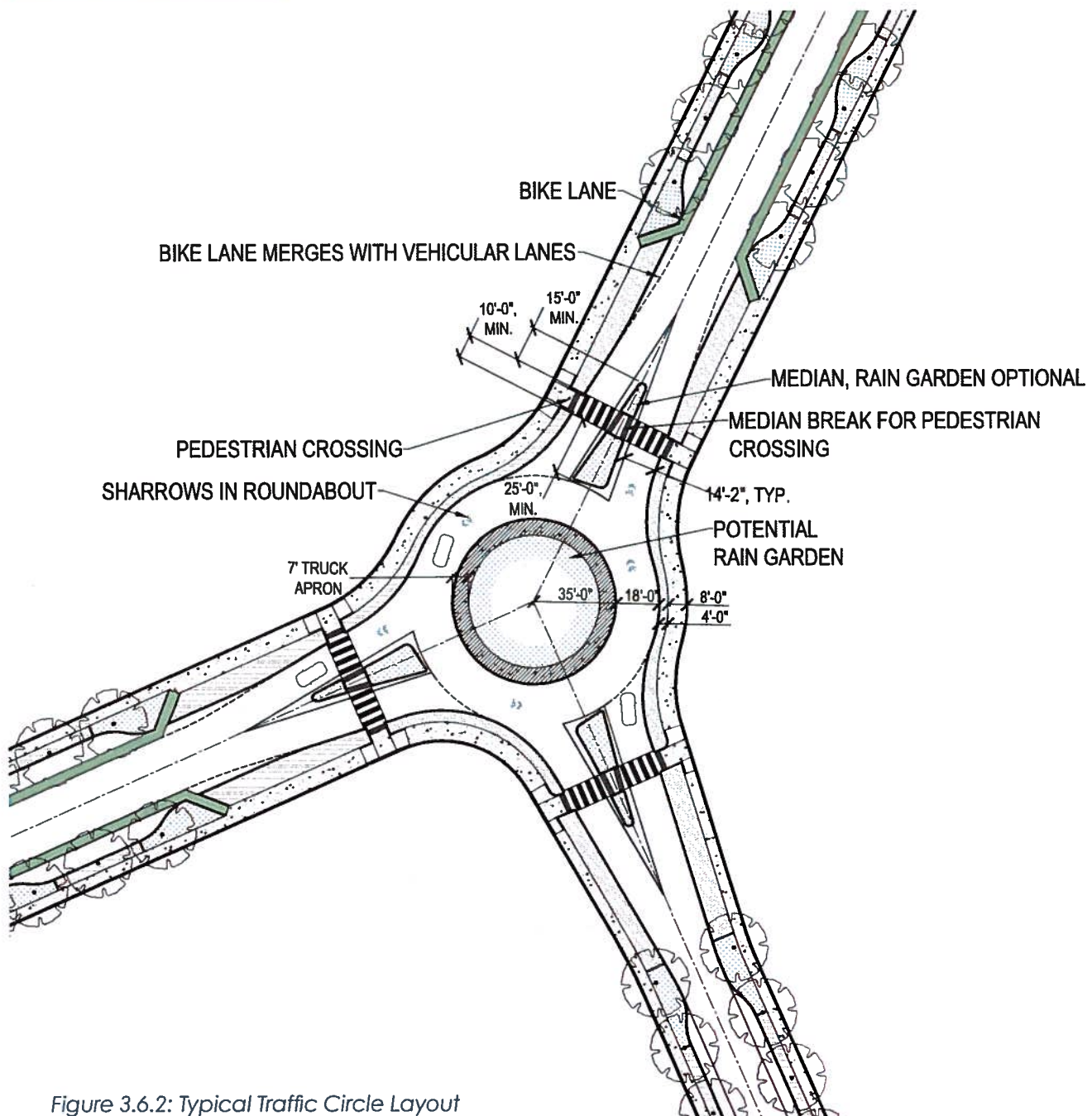


Figure 3.6.2: Typical Traffic Circle Layout

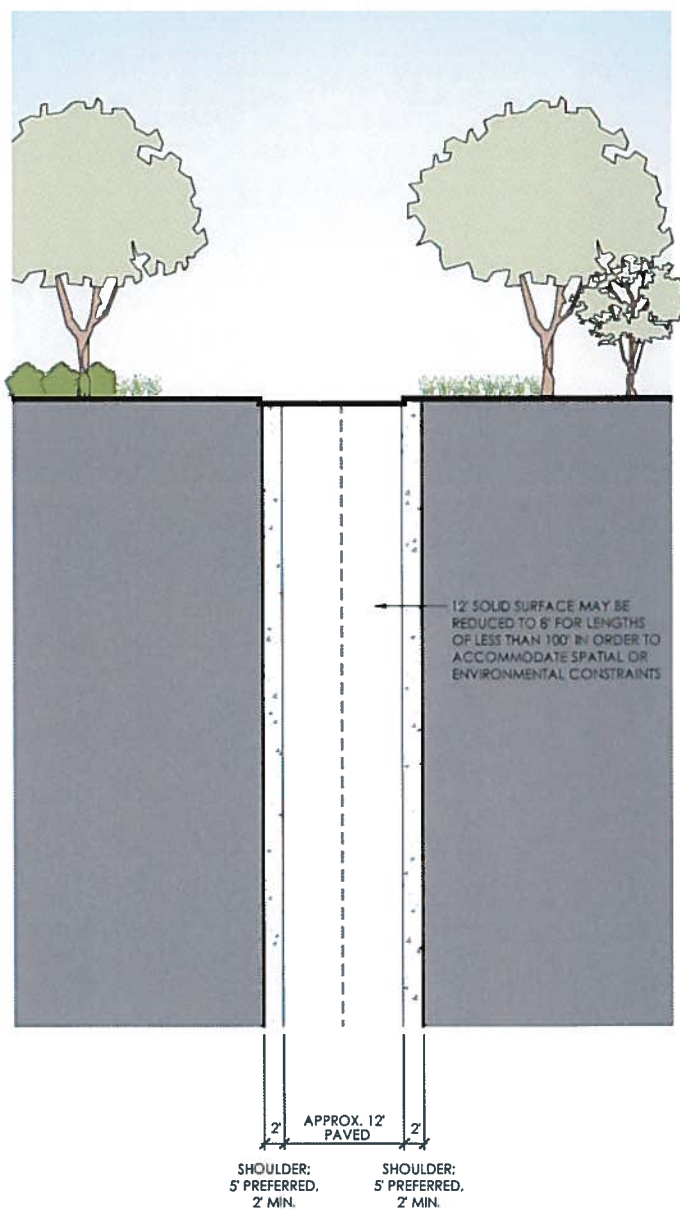
3.7 Trail Requirements

Trails at The Grove at Shoal Creek include the Shoal Creek Trail and North Greenbelt Trail as well as the trails along Bull Creek Road and the Green Streets, which are described in Sections 3.4 and 3.5. Section 3.7.1 defines requirements for the trails listed above, while 3.7.2 provides additional requirements for the Shoal Creek Trail. These requirements are intended to apply only to the urban trails on the site and do not apply to soft-surface trails, sidewalks, paths, and secondary trails within the park

3.7.1 The following requirements apply to all urban trails on the site:

- Unless specifically noted otherwise, these trails shall follow the requirements of the City of Austin Urban Trail Master Plan.
- While a 12' width is desired for all trails, a width of 8' is acceptable for trails, other than the Shoal Creek Trail and Bull Creek Road Trail. The width of any trail may be reduced to 8' for a length of up to 100' to accommodate spatial or environmental constraints.
- All trails within the development shall include wayfinding elements that describe distance, direction, and destination, at intervals of $\frac{1}{4}$ to $\frac{1}{2}$ mile. The purpose of these wayfinding elements is to orient users and visitors to the trail's destination, provide educational or informational background on the site, and facilitate recreational use (e.g. mile markers).
- Multi-use Trails should have a minimum centerline radius of 100'. Centerline radii where approaching curb ramps at intersections, road crossings, street islands, etc. should be no less than 10'.
- Raised street crossings should have a level surface that is the same width as the multi-use trails. The crossing surface should be 3" above the adjacent roadway with a 6' long transition to the road surface on either side. Where site drainage patterns do not allow for raised crossings, this geometry may be adjusted with approval from the City of Austin.

3.7.2 Requirements for the Shoal Creek Trail



NOTES:
1. TRAIL SEGMENTS LESS THAN 12' IN WIDTH FOR LENGTHS OF GREATER THAN 100' MAY BE ALLOWED WITH APPROVAL FROM PUBLIC WORKS.

Figure 3.7.2: Shoal Creek Trail

3.8 Parking Requirements

It is important that The Grove at Shoal Creek provide ample parking to meet the needs of the project's users. The majority of the site's parking needs will be met in parking garages, residential garages, and with on-street parking within the site. Some off-street surface parking may be necessary to ensure the viability of specific retail uses. Off-street surface parking is defined as a vehicle parking lot consisting of at least 10 spaces where neither the parking space themselves nor the drive isle serving the spaces is located within the street zone. Off Street Surface Parking does not include parking in residential driveways. Parking for the Grove shall meet the following standards:

3.8.1 Off-street surface parking may not cumulatively exceed 400 spaces for the entire site. Compliance with this standard shall be determined at final site plan and shall not apply to prior site plans.

3.8.2 Off-street surface parking should generally be located beside or behind buildings and should not occur between a building section and its Primary Frontage as described in Section 4 of this document.

3.8.3 Off-street surface parking shall be constructed to meet or exceed City of Austin requirements for parking lot landscaping.

3.8.4 Off-street surface parking lots are encouraged to be designed such that the paved surface drains into landscaped parking islands and peninsulas.

3.8.5 Required ADA parking shall be no more than 250' from the site it is serving.

3.8.6 Parking on the site shall not cumulatively exceed the parking requirements of Appendix A of the Land Development Code. Where a site plan includes a structured parking facility intended to serve future phases, the portion of that facility that exceeds parking requirements for that site plan must be barred from use until the future phase which it serves comes on-line.

3.8.7 Unless otherwise noted in this document, requirements of the City of Austin Land Development Code and Transportation Criteria Manual shall apply to parking in the project, including requirements regarding ADA parking, off-site parking, and design and construction standards.

4.0 ARCHITECTURE

4.1 Intent

Section 4 of The Grove at Shoal Creek Design Guidelines establishes standards and requirements for placement and design of buildings within the site. They are meant to ensure that buildings contribute to creating a walkable/ pedestrian scaled neighborhood and to establish the relationship/ placement of buildings to the variety of streets within the Grove at Shoal Creek. It is not the intent of this section to mandate any particular architectural style or character or to unduly limit creative expression. The intent of this section is to:

4.1.1 Ensure that buildings relate appropriately to surrounding uses and streets and create a cohesive visual identity and attractive pedestrian friendly streetscape.

4.1.2 Provide appropriate architectural direction to create a high-quality community development and streetscape environment.

4.1.3 Provide for a strong physical relationship between buildings and adjacent streets and sidewalks. Provide for convenient and easy pedestrian access to buildings

4.1.4 Provide design flexibility in building placement standards to allow for unique and diverse architectural expressions as well as for pedestrian-scaled uses such as outdoor dining terraces, porches, patios, and landscape features to enliven and enrich the streetscape environment.

4.1.5 Encourage buildings with appropriate human and pedestrian scale that create a sense of community. Building Architectural elements will be encouraged to help create gateways and public spaces and identify key intersections.

4.1.6 Encourage appropriate use of glazing, shading, and shelter to ensure that buildings contribute to the creation of a pedestrian-friendly environment with an active ground-floor experience.

4.1.7 Provide the flexibility necessary for diverse and well-articulated buildings throughout the site. Standards should encourage rather than hinder architectural creativity and expression.

4.2 Using This Section

Standards in this section are provided for the two Development Districts identified in Section 2: The Mixed-use District and the Residential District. If designing a building within a Tract where both districts are allowable, it will be up to the design team to determine which district is most appropriate for each building or site. For buildings where 10% or more of the gross square footage is dedicated to retail or office uses, and buildings where the primary frontage is the Retail Street, the Mixed-Use District must be used. Otherwise, this decision is fully at the discretion of the design team.

Standards for building placement are given in relationship to the Street Zone, Greenway Zone, or adjacent Park Space. Many building sites will be surrounded on three or more sides by such zones. For each building or site, it will be at the discretion of the design team to determine which of these is the Primary Frontage for the project. A Street Zone, Greenway Zone, or Park Space, may be selected to serve as the Primary Frontage. However, for sites bounded by the Retail Street, the Retail Street must serve as the Primary Frontage.

Some standards are given in relationship to the Bull Creek Road right-of-way that will apply regardless of whether that is selected as the project's Primary Frontage.

4.3 Building Placement

4.3.1 Mixed-Use District

- a. Buildings may be constructed immediately adjacent to the Street Zone, Greenway Zone, or Park Space. There is no minimum setback.
- b. The maximum setback for buildings along their Primary Frontage is 30', unless a public plaza is provided between the building and primary frontage.
- c. At least 50% of a site's Primary Frontage must consist of continuous building façade constructed within the maximum setback described in 4.3.1.b.
- d. Shade structures and canopies are permitted to encroach into the Street Zone or Greenway Zone above 12 feet of height to provide shade and architectural interest. There is no limitation to the distance which shade structures and canopies may encroach into the Street Zone or Greenway Zone, and support posts are allowed within the Street Zone or Greenway Zone as long as they do not interfere with the required sidewalk. Shade structures and canopies shall not interfere with street trees at maturity.
- e. Occupied space in buildings above the first floor is permitted to encroach into the Street Zone or Greenway Zone above 12 feet of height to increase the developable area of the structure and provide architectural interest. This type of encroachment may be a maximum of 7' or 10% of the width of the combined Street and Greenway Zone, whichever is smaller. Buildings in the street zone shall not interfere with street trees at maturity.
- f. Buildings may not encroach into Park Space.
- g. Off-street surface parking is not permitted between the building and the Primary Frontage. A slip road is allowed in these locations (ref. section 3.5.8). (Note: off-street surface parking is allowed between the building and other Street Zones, Greenway Zones, or Park Spaces not selected as the Primary Frontage).
- h. Off-street surface parking is not permitted between any building and the Bull Creek Road right-of-way regardless of whether Bull Creek Road is the Primary Frontage of the site. A slip road is allowed in these locations (ref. section 3.5.8).
- i. For sites bounded by Bull Creek Road at least 50% of the site's Frontage on Bull Creek Road must consist of continuous building façade constructed within the maximum setback described in 4.3.1.b, regardless of whether Bull Creek Road is selected as the Primary Frontage. A slip road is permitted between the site and Bull Creek Road (ref. section 3.5.8).

4.3.2 Residential District

a. Buildings in the Residential District may not encroach into the Street Zone, Greenway Zone, or Park Zone.

b. Detached residences:

1. The minimum setback from the Primary Frontage is 10'. The minimum setback for porches or stoops is 5'.

2. The minimum setback for front-facing garages is 18'. Parking is allowed in the driveway of a front-facing garage.

3. The maximum setback for the Primary Frontage is 25'.

4. Tandem parking is permitted.

c. Attached Residences (e.g. Townhomes, Row Homes, Duplexes, Multifamily Buildings)

1. The minimum setback from the Primary Frontage is 5' for the first floor only to allow for porches and stoops. There is no minimum setback above the first floor.

2. There is no minimum setback for porches, stoops, balconies, etc.

3. The maximum setback from the Primary Frontage is 30'.

4. At least 50% of a site's Primary Frontage must consist of continuous building façade constructed within the maximum setback described above.

5. Front-facing garages are generally discouraged but will be allowed where necessary. The setback for front-facing garages is 5'. Parking is allowed in the driveway of a front-facing garage so long as that garage is set back a minimum of 18' from the Street Zone.

6. Tandem parking is permitted.

7. Off-street surface parking is not permitted between the building and the Primary Frontage. (Note: off-street surface parking is allowed between the building and other Street Zones, Greenway Zones, or Park Spaces not selected as the Primary Frontage).

8. Off-street surface parking is not permitted between any building and the Bull Creek Road right-of-way regardless of whether Bull Creek Road is the Primary Frontage of the site.

9. For sites bounded by Bull Creek Road at least 50% of the site's Frontage on Bull Creek Road must consist of continuous building façade constructed within the maximum setback described in 4.3.2.c.3 regardless of whether Bull Creek Road is selected as the Primary Frontage.

4.4 Building Design Standards

4.4.1 General Design Standards

All buildings at The Grove shall meet the following standards:

- a.** Generally, pedestrian entries to the buildings are encouraged as frequently as practical along all Street Zones, Greenway Zones and Park Space frontages. At least one primary pedestrian building entry must occur along the building's Primary Frontage.
- b.** Ground floor residential units that are oriented toward the street should have direct access from the street where practical, via porch, stoop, or other entries. At a minimum, 50% of such units shall have direct entries from the street. Where feasible, Elevation of ground floor units should be slightly elevated above the sidewalk elevation. A range of 12-36 inches is considered optimal and should be utilized where feasible. However, site grading constraints may result in a wider range of acceptable ground floor elevations. Where these conditions exist, the building or first floor should generally be set back sufficiently from the Street Zone to allow for a porch, stoop, terrace, or other pedestrian access.
- c.** Ground floor retail uses shall generally have a height and depth sufficient to support the intended use and shall have at least one pedestrian entry along the street they are oriented towards.
- d.** Parking structures, when utilized, should be designed to not dominate the built environment, and should be visually screened from the street through use of architectural treatment or green screens. Where possible, wrapping parking structures with buildings is encouraged.
- e.** Building cladding materials shall be high quality and attractive. Preferred materials include Texas limestone or sandstone, smooth horizontal bevel or lap-siding fiber-cementous siding with mitre corners, smooth finish or painted brick, smooth finish stucco; or other similar or special materials where appropriate and complimentary to the overall context and character.

4.4.2 Mixed-Use District Standards

- a.** Medium Density residential and commercial mixed-use building are strongly encouraged – they should be designed to extend and enliven the fabric of the streets. These mixed-use buildings and uses are not intended to be stand-alone buildings but an integral part and core of The Grove at Shoal Creek community creating ground level activity and neighborhood oriented uses. They will provide a scale transition to adjacent Townhomes and Single family districts of the master plan.
- b.** For buildings whose primary frontage is the Retail Street, at least 70% of the primary frontage shall consist of pedestrian oriented uses, including retail, lobbies serving office uses, and lobbies, sales centers, or amenity areas serving residential uses. Buildings facades along the Retail Street that exceed 200 ft in length shall have a building entrance at least every 100 ft.
- c.** Mixed-use buildings are encouraged to be designed with pedestrian friendly outdoor elements such as extended/projecting eaves for shade and the use of loggias, porches, terraces, and/or courtyards.

d. For all uses in a Mixed-Use building, the minimum off-street parking requirement shall be 60 percent of that prescribed by the City of Austin Off-Street Parking and Loading Requirements. This reduction may not be used in combination with any other parking reduction.

e. Glazing

1. For non-residential uses on the ground floor along a building's Primary Frontage, at least 30% of the wall area of the first floor between 0 and 12' must consist of glazing.
2. For residential uses on the ground floor along a building's Primary Frontage, at least 10% of the wall area of the first floor between 0 and 12' must consist of glazing.
3. Along a building's Primary Frontage, at least 10% of the wall area for the second floor (if provided) must consist of glazing.
4. Where a building faces any Street Zone, Greenway Zone, or Park Space that is not its Primary Frontage, at least 10% of the wall area of the first two floors must consist of glazing unless building code prevents windows on such facades.
5. These glazing standards do not apply to parking structures.

4.4.3 Residential District Standards

a. Detached Residence

1. On the front façade, at least 10% of the wall area of the first floor of detached residences must consist of glazing. The area of a front facing garage door is not counted in this calculation.
2. Use of porches, terraces, and other front-facing outdoor spaces is strongly encouraged.
3. Homes on corner lots should be designed so that architecturally attractive elevations are presented to both sides by using such elements as wrap-around porches, landscape design elements, massing, façade composition, and other design elements. If necessary when a garage faces a side elevation on a corner it shall be designed as an extension of the primary elevation.

b. Attached Residences / Multi-family Buildings

1. Grouping of townhomes/row homes shall have a minimum separation of 10 ft every 180 ft or 8 units whichever is less. This separation shall allow for pedestrian access and circulation to/from alleys and through the neighborhood.
2. Townhomes/Row Houses on corner lots shall be designed and situated so that both street frontages are front facades; with corner elements and architectural compositions encouraged to create handsome facades on both sides.
3. Multi-family buildings are encouraged to be designed with pedestrian friendly outdoor elements such as extended/projecting eaves for shade and the use of loggias, porches, terraces, and/or courtyards.

4. Where a building faces any Street Zone, Greenway Zone, or Park Space, at least 10% of the wall area of the first two floors must consist of glazing unless building code prevents windows on such facades. The area of a garage door is not counted in this calculation.

c. Residential Development Adjacent to Public Parks

1. Where residential development abuts public parkland, the development shall meet the standards depicted in Figure 4.4.3.

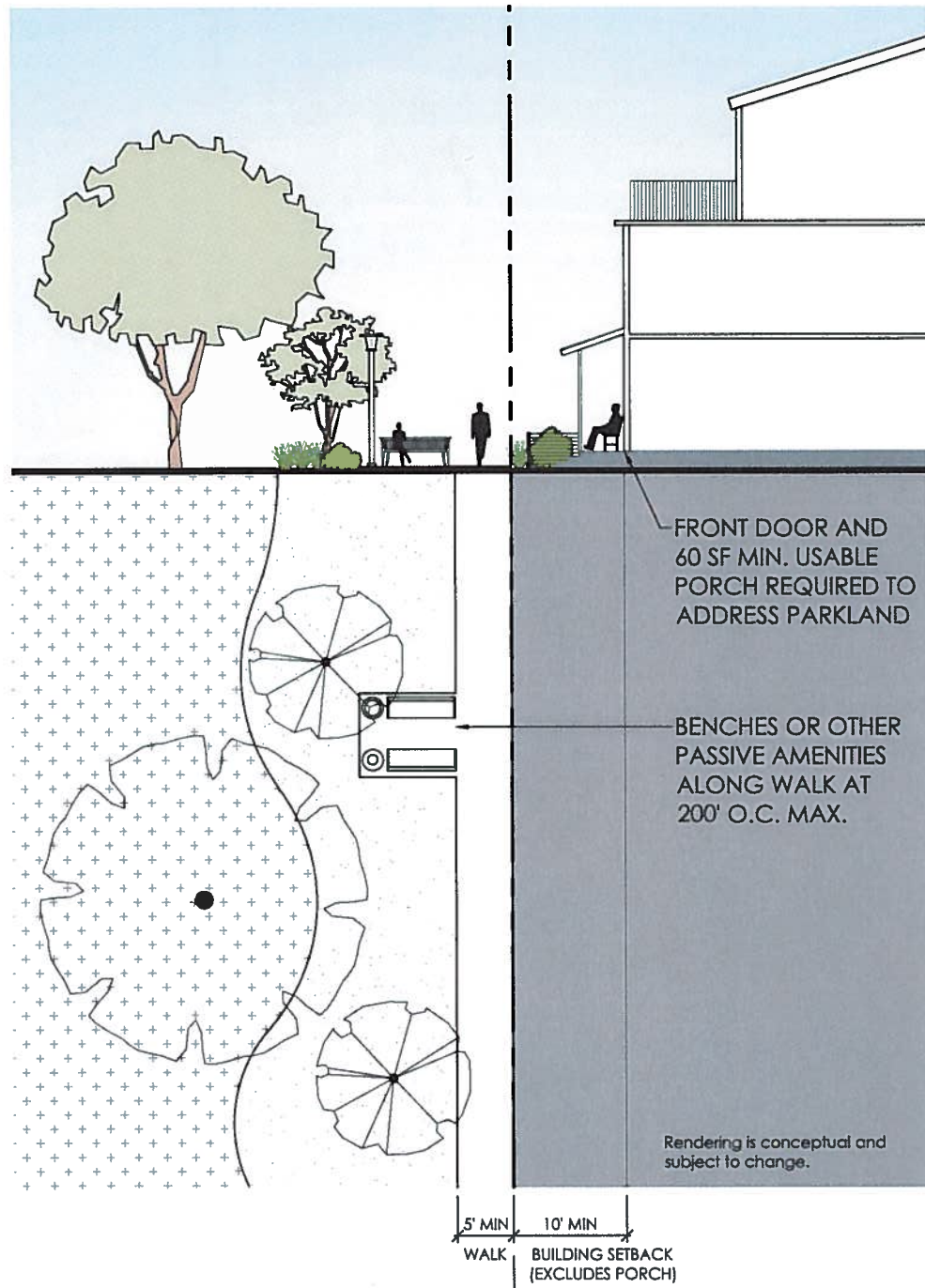


Figure 4.4.3: Residential Development on Parks

5.0 Landscape and Open Space

5.1 Intent

Section 5 of The Grove at Shoal Creek Design Guidelines establish standards to ensure that the landscape, park space, and open space elements within the project support the overall Guiding Principles of The Grove. Landscape elements throughout the project will be implemented in ways that enhance experience of the urban spaces, create high quality parks, and support environmental sustainability. The intent of this section is to:

5.1.1 Ensure the creation of high quality parks and common open spaces for the enjoyment of residents and visitors alike.

5.1.2 Ensure that the landscape within the streetscapes of The Grove provides shade as well as a quality environment.

5.1.3 Ensure screening of equipment and utilities.

5.1.4 Provide standards for lighting within The Grove to minimize off-site impacts.

5.1.5 Provide standards for signage within The Grove, allow signage as advertisement to support economic sustainability, and encourage signage that is pedestrian scaled and supportive of the urban vision for The Grove.

5.2 Parks and Open Spaces

5.2.1 Introduction

The park spaces throughout the site are meant to provide a variety of uses and activities to serve the area neighborhoods and create space for multi-modal and sustainable infrastructure. Park Space includes both publicly dedicated and privately owned but publicly accessible open spaces. As shown on the PUD Parks Plan (Figure 5.1), the park spaces will consist of:

- The Signature Park (16.25 acres minimum) along Shoal Creek
- A public plaza within the Mixed-Use District
- A Central Greenbelt connecting the Residential and Mixed-use Districts (ref. Framework section)
- A North Greenbelt connecting Bull Creek Road to the Signature Park (ref. Framework section)
- A Pocket Park along Bull Creek Road

5.2.2 Signature Park

The Signature Park will be the largest park at The Grove and will house most of the site's mature oak trees. The following guidelines should be used in developing plans for the Signature Park:

- The park character should evolve from an urban, active edge on its west end to a restored natural area with trails and enhanced native prairie and grow zone on its east end, as it approaches Shoal Creek.

- Amenities within the Signature Park should include, at a minimum: a children's playscape, paved trails, soft-surface trails, a wet pond with overlooks and picnic areas, an open lawn area and the Shoal Creek Trail on the eastern edge of the project.
- Outside of trails and necessary parking, large areas of paving should generally be avoided in the Signature Park.
- Great care should be taken in preserving the existing trees in the Signature Park. While it is important to provide park users with access to the trees and the shade they provide, care should be taken when planning or constructing improvements within the critical root zone of existing trees.

5.2.3 Central Plaza

The Central Plaza will be the central urban gathering place within the project. The following guidelines should be used in developing plans for the Central Plaza:

- Provide plenty of shade with shade structures and/or shade trees.
- While heavy use will dictate large paved areas in the plaza, ensure green spaces are integrated as frequently as practical.
- Outdoor dining should be encouraged for restaurant uses surrounding the plaza. Kiosks are also encouraged.
- An interactive water feature is encouraged within the plaza.

5.2.4 Bull Creek Road Pocket Park

Situated around a grove of mature live oaks, the Bull Creek Road Pocket Park will provide a welcoming entrance into the residential portion of the site off of Bull Creek Road as well as a neighborhood amenity for the site and nearby neighbors. The following guidelines should be used in developing plans for the Bull Creek Road Pocket Park:

- Open lawn space for passive uses should predominate the park
- Amenities may include a picnic pavilion, a small children's play area, a garden area, sidewalks, and trails.
- Great care should be taken in preserving the existing trees in the Pocket Park. While it is important to provide park users with access to the trees and the shade they provide, care should be taken when planning or constructing improvements within the critical root zone of existing trees.

5.2.5 Additional Pocket Parks

Additional pocket parks may be included throughout the site to provide small amenity and gathering spaces near homes and places of business. Where included, these pocket parks should generally be at least 10,000 SF in size and should include a range of passive amenities, which may include:

- Open lawn
- Gardens
- Seating and picnic areas
- Small gazebos or shade structures
- Small gathering spaces

5.2.6 Other Green Spaces

Throughout the site there may be additional, dispersed green spaces. These spaces should take a form and character that complements the context in which they lie. Raingardens and other green infrastructure are encouraged to be included where feasible and appropriate within the overall drainage of the site.

5.2.7 Greenbelts and Trails

Reference Section 3

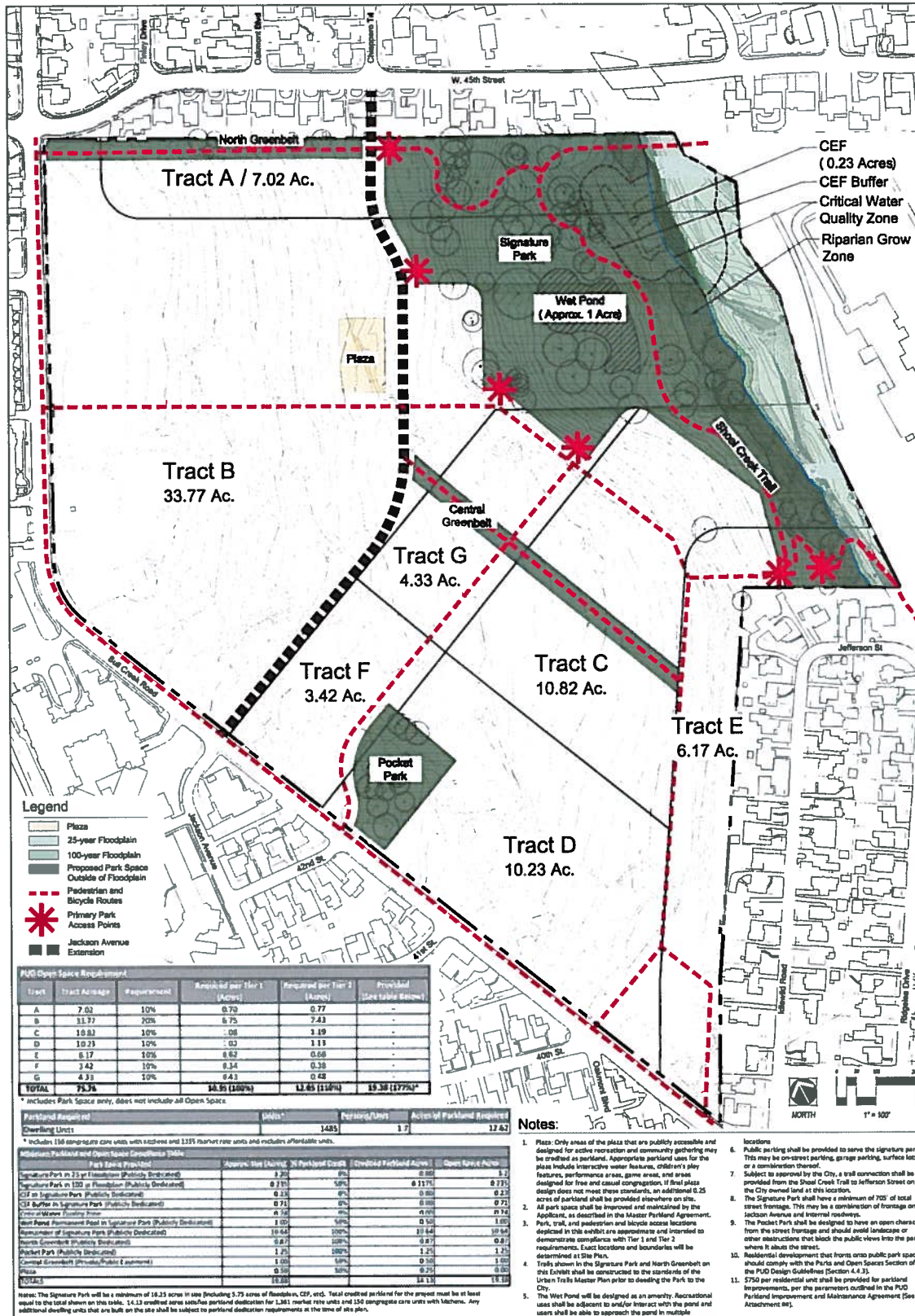


Figure 5.1: PUD Parks Plan Exhibit

5.3 Landscape and Streetscape

5.3.1 Landscape Character and Plant Selection

a. Landscape applications should complement the context in which they are located. For example, landscape in an urban setting may include more intensive treatments. Landscape in the parks and green spaces should have a natural look with lawns and organic planting patterns among the existing trees. Landscape in the residential areas should focus on creating a sense of safety and community with well maintained and diverse plantings.

b. Native and drought tolerant plant species should be used as much as possible. A minimum of 95% of non-turf plant materials on any project should be from the Austin Grow Green Guide or should demonstrate equal appropriateness to the Austin environment.

5.3.2 Street Trees

a. Street trees shall be provided as specified in the Framework Section. Spacings specified are intended to be approximate and may vary based on infrastructure, intersections, driveways, utilities, etc.

b. Street trees shall be a minimum of 3" caliper measured 6" above the base at the time of planting. Street trees may be counted toward requirements for mitigation of existing trees.

c. Street tree species should vary throughout the site. While a single street or project may contain a monoculture of trees, no single species should represent more than 25% of street trees planted at The Grove. This requirement is intended to apply to the site as a whole and should not apply to any one street, project, or site plan.

5.3.3 Green Infrastructure

a. Raingardens, bioswales, and other green infrastructure elements shall be designed and landscaped to create a well-maintained and visually appealing character.

b. Green infrastructure elements shall be planted in accordance with the City of Austin Environmental Criteria Manual, in effect on the date of approval of these guidelines.

5.3.4 Tree preservation and replacement

a. Tree preservation for this project is dictated by the approved PUD Ordinance.

b. All healthy, non-invasive trees on site should be preserved to the extent feasible, unless those trees are creating a negative impact on higher value trees (e.g. located too closely together causing competition for space and nutrients). Removal and mitigation of these trees is governed by the Tree Disposition Plan attached to the PUD.

c. Preservation of trees shall be in accordance with the City of Austin Code and Environmental Criteria Manual.

5.3.5 Street Furniture and Paving

- a. Furnishings such as benches, trash and recycling receptacles, etc should be placed within the high pedestrian traffic areas and transit stops at intervals which ensure convenience and comfort.
- b. The aesthetics of the site furnishings should create a cohesive theme throughout the project. But may vary depending on context. For example, furnishings may be a more contemporary style in the urban plaza and a more classic style along trails in the signature park.
- c. Special pavement applications are encouraged in plaza areas, other specialty pedestrian areas, and may be used to help delineate pedestrian crossing. Permeable pavers or porous pavements may be considered where possible.

5.3.6 Screening of Equipment and Utilities

- a. All mechanical equipment and utilities, with the exception of solar panels, shall be screened from view from the streets by either landscaping or decorative enclosure.
- b. Loading docks, truck parking, outdoor storage, trash collection, trash compaction, and other service functions shall be incorporated into the overall design of the building and landscape so that the visual impacts of these functions are contained and out of street-level view from adjacent streets and street zones. Screening materials for solid waste collection and loading areas shall be the same as, or of equal quality to, the materials used for the principal building. These functions may be placed along commercial alleys without the necessity of screening from the alley.

5.3.7 Walls and Fences

Fencing is allowed on site and is generally encouraged where necessary to define private spaces and create necessary boundaries between uses. Fencing in the residential zone shall meet the following standards:

- a. Fences or walls located at the sides or backs of buildings are permitted and may be up to 7 feet in height. These fences shall be constructed of wood, decorative metal, masonry, or other quality materials.
- b. Fences located between the front of buildings and the street zone are allowed in the Residential Zone only to define private front yard spaces. These fences must be no greater than 4' in height and must be constructed of wood, decorative metal, masonry, or similar quality material. Height limit is not inclusive of any retaining walls.

5.4 Exterior Lighting

5.4.1 Street and Area Lighting

Lighting is an important component to site safety. Street lighting should provide light for both the vehicular lanes and pedestrian sidewalks. Lighting along pedestrian paths and within parks should meet minimum safety standards in all locations where night use is expected.

All site and area lighting shall limit off-site impacts by meeting the following requirements based on the International Dark-Sky Association / Illuminating Engineering Society Joint Model Lighting Ordinance published in 2011, and utilizing the BUG rating system. The BUG rating system consists of three components: B (Backlight), U (Uplight), and G (Glare). The following requirements are for all site and area lighting fixtures on site:

- a. The maximum allowable Uplight rating shall be U2. Fixtures that do not have a BUG rating but are rated as Full Cut-off shall be assumed to be in compliance with this requirement.
- b. For fixtures located less than 2 mounting heights from the boundary of the The Grove at Shoal Creek, the maximum Backlight rating shall be B2.
- c. For fixtures located less than 2 mounting heights from the boundary of The Grove at Shoal Creek, the maximum Glare rating shall be G1.
- d. Where the site abuts Bull Creek Road, the centerline of the road shall be considered the boundary of The Grove at Shoal Creek for the purposes of determining compliance with the above requirements.

5.4.2 Accent Lighting

Lighting is also a useful tool for enhancing architectural and landscape aesthetics and enjoyment of a site. Accent lighting should be utilized to highlight trees, architectural elements, landscape elements, artwork, and other unique features as appropriate, especially in the public plaza and along the Retail Street. The following regulations will govern accent lighting:

a. Directional Luminaires

Directional Luminaires may be used to illuminate signs and flagpoles. Such luminaires shall be installed and aimed so that they illuminate only the specific object or area and do not shine directly onto neighboring properties or roadways.

b. Landscape Lighting

Uplighting and downlighting of trees, artwork, kiosks, and other landscape features shall be allowed. Landscape lighting fixtures must be 24 volts or less unless they are directed downward and shielded.

c. Lighting of Building Facades

1. Downlighting of buildings and structures is permitted if fixtures are fully shielded or full cut-off or if they meet the requirements below for Uplighting.
2. Uplighting of building facades should only be used to highlight specific architectural features such as principal entrances, corners, terminus elements, and towers, and allowed in the Mixed-use District only. Luminaires used for uplighting are limited to 100 Lumens per linear foot of façade to be lit (measured horizontally), unless the fixture is 24 volts or less.
3. Direct view fixtures are permitted in the Mixed-use District on building facades and are limited to 250 lumens per linear foot of fixture.

d. Festoon Lighting

String lights and festoon lighting are permitted over roadways and in outdoor use areas within the Mixed Use District as temporary or permanent installations.

5.5 Signage**5.5.1 Free Standing Signs****a. Community Identity Signs**

For each vehicular entry to the project depicted on the Land Use Plan, two permanent subdivision identification signs with a combined sign area of not more than 128 square feet and height not exceeding 6 feet are permitted. One additional sign with a sign area of not more than 64 square feet and height not exceeding 6 feet is permitted at the northwest corner of the site near the intersection of Bull Creek Road and 45th Street.

b. Commercial Multi-tenant Signs

Up to two (2) multi-tenant signs are allowed for The Grove at Shoal Creek development. These signs are subject to the following standards:

- A maximum area of 250 square feet
- A maximum height of 20'

c. Project Identity Signs

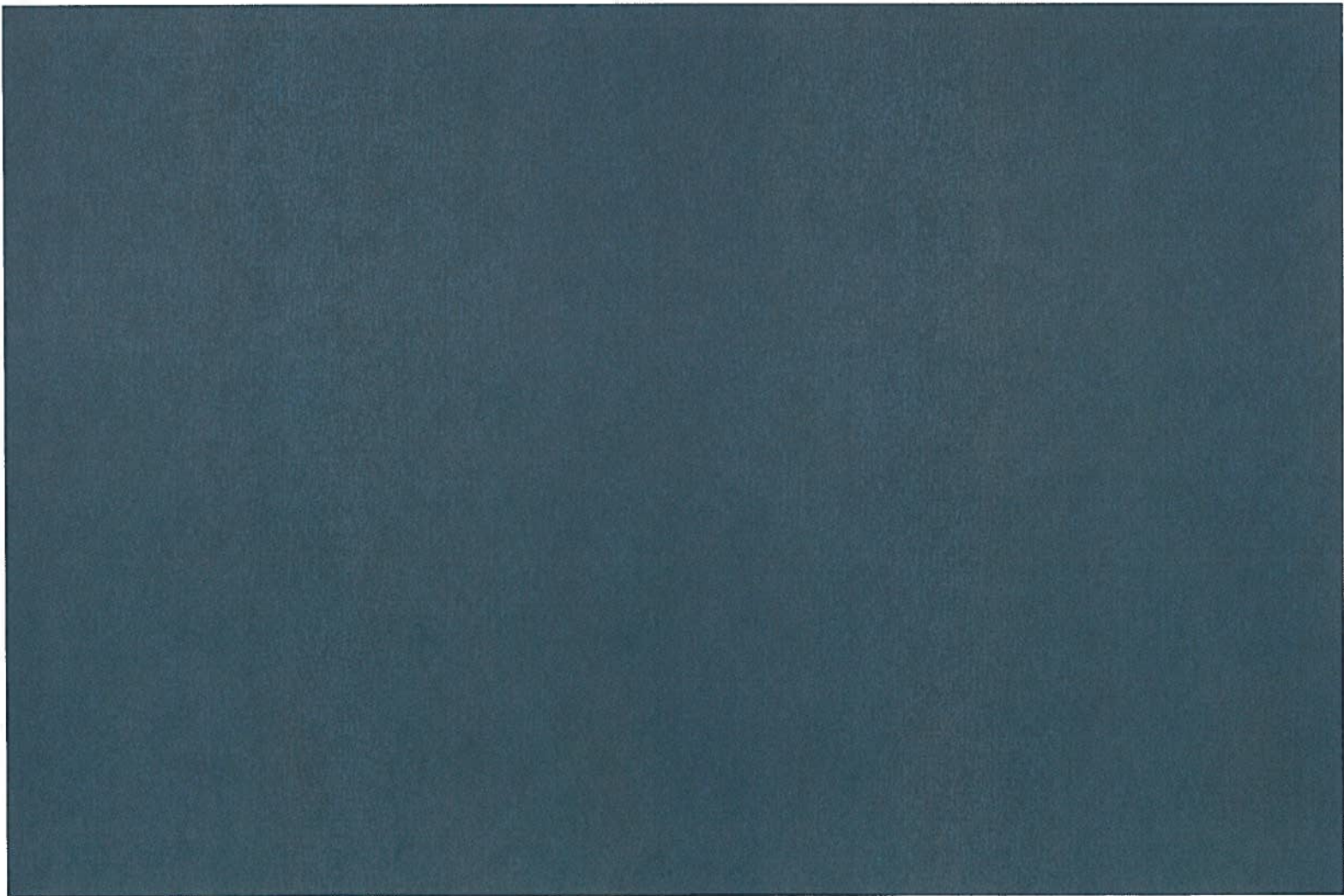
For each building containing a non-residential use or more than one residence, a free standing sign is permitted on the same lot. This sign shall not exceed 35 sf in area or 6' in height.

5.5.2 Building Signs

Building signs are permitted on all buildings within The Grove except detached single family residences. Blade signs, awning signs, under-canopy signs, heraldic signs, and letter-mounted signs are encouraged. The total sign area on any building shall not exceed 20 percent of the façade area of the first 15 feet of the building.

5.5.3 Non-permanent Signs

Signs such as commercial flags and street banners add vibrancy and character to the street scene and reinforce community events and programs. These temporary signs are permitted within the boundaries of The Grove at Shoal Creek without restriction.



Summary of Changes to LUP and Design Guidelines

The Grove at Shoal Creek
October 24, 2016

The following document describes changes made to The Grove at Shoal Creek zoning application since the last Formal Submittal on March 28, 2016. The source or reason for the change is provided in parenthesis after each item. Some changes are simply minor corrections and clarifications and are marked as such. Changes to the Design Guidelines listed here have been outlined or underlined in red throughout that document.

Tier Compliance Summary

1. Updated open space references to indicate 19.38 instead of 18.12 acres. (*Condition of PARD Superiority*)
2. Modified language regarding trees to change "poorer quality" to "fair to poor condition" for better consistency with code language. (*Heritage Tree Foundation Request*)
3. Updated Tier 2 Item G to increase the requirement to a minimum of 8 car share spaces. (*ZAP Recommendation*)
4. References to "shared access improvements" (points B.3(v) and G(v)) have been removed from the Tier 1 compliance table. (*Clarification*)

Code Modifications

1. Modified code modification of 25-8-641 to specifically list the tree numbers that may be removed and to exempt the trees to be saved from the administrative variance for reasonable use process. (*Heritage Tree Foundation Request*)

Land Use Plan

Sheet 1

1. The proposed Jackson Avenue alignment has been graphically updated to reflect the most likely alignment. (*Clarification*)
2. The wording of note 6 on Sheet 1 has been updated to clarify that the Green Water Quality controls distributed on-site must provide initial treatment but each feature need not be sized to treat its entire drainage area (additional treatment would be provided later in sequence). (*Clarification*)
3. Note 7 was added on Sheet 1 to require that on-site drainage systems be designed to the 100-year storm. (*Commitment at Environmental Commission*)

Sheet 2

4. The Open Space uses table has been updated to clarify that these uses are permitted in all tracts and that Community Recreation and Parks and Recreation Services are permitted uses under Open Space. (*PARD Request*)
5. Single-family detached uses were removed from Tracts F and G. (*ZAP Recommendation*)
6. Group Residential has been added as a permitted use in Tracts B, F, and G. (*ZAP Recommendation*)
7. Live-Work Units have been added as permitted uses in Tracts B, F and G. (*Clarification*)

8. A note has been added to the Permitted Use Tables to restrict single-family detached residential uses in Tract B to the area within 150' of Tract A. *(ZAP Recommendation)*
9. Note 2 on Sheet 2 has been updated to clarify that residential units are capped at 1335 (not including affordable housing), that Congregate Care beds are capped at 300, and to define affordable housing. *(Staff Recommendation)*
10. Note 4 on Sheet 2 was updated to reduce the maximum office square footage to 210,000 SF. *(Staff Recommendation)*
11. Note 5 on Sheet 2 was updated to reduce the maximum retail square footage to 150,000 SF. *(Staff Recommendation)*
12. Note 16a on Sheet 2 was updated to clarify that the fiscal fees for the bridge may only be used by the City on bicycle and pedestrian improvements located east of the property in the event that the bridge cannot be constructed. *(ZAP Recommendation)*
13. Note 17b on Sheet 2 was updated to increase the required number of car-share parking spaces to 8. *(ZAP Recommendation)*
14. Note 18 was added to Sheet 2, which caps overall development on the site at 2.65 million square feet *(ZAP Recommendation)* and also includes the other staff conditions to their recommendation for PUD approval. *(Staff Recommendation)*
15. Note 6 was added to the Site Development Regulations table to require a 25' minimum setback from Bull Creek Road on Tract D. *(ZAP Recommendation)*
16. The Summary of Development Entitlements Table was added to Sheet 2 to provide a better reference point for the various entitlement caps agreed to as part of the PUD application. *(Clarification)*

Parks Plan Exhibit

1. A proposed Jackson Avenue alignment has been added graphically to the exhibit. *(Clarification)*
2. The signature park has been extended up to the proposed public right-of-way along Jackson Avenue in Tract A and the northern park of Tract B. *(Condition of PARD Superiority)*
3. The bike and pedestrian trails shown through the Signature Park have been updated to better reflect the anticipated alignment designed to minimize impacts to the CRZ's of the heritage oaks. *(Clarification)*
4. The open space provided had been increased by 1.26 acres (from 18.12 to 19.38 acres) and now exceeds the requirement by 77%. *(Condition of PARD Superiority)*
5. Total park space acreage has increased by 1.25 acres (from 18.63 to 19.88). *(Condition of PARD Superiority)*
6. Credited parkland acres has increased by 1.25 acres (from 12.88 to 14.13). *(Condition of PARD Superiority)*
7. Flex Park Space acreage has been removed from the Park Space Provided table. *(Condition of PARD Superiority)*
8. The minimum Signature Park acreage has increased by 3.25 acres (from 13.00 to 16.25). *(Condition of PARD Superiority)*
9. A note has been added to the Park Space Provided table regarding superiority and congregate care units. *(Condition of PARD Superiority)*
10. The note regarding Flex Park Space has been removed and any references to Flex Park Space have been removed from the other notes. *(Condition of PARD Superiority)*
11. Note 7 regarding the trail connection to Jefferson has been added. *(Condition of PARD Superiority)*

12. Note 8 has been added to state that the Signature Park will have a minimum of 705' of street frontage. *(Condition of PARD Superiority)*
13. Note 9 regarding parameters for the Pocket Park has been added. *(Condition of PARD Superiority)*
14. Note 10 regarding residential development along public parks has been added. *(Condition of PARD Superiority)*
15. Note 11 regarding the parkland improvement fees has been added. *(Condition of PARD Superiority)*

Framework Plan

1. The proposed Jackson Avenue alignment has been graphically updated to reflect the most likely alignment of that road. *(Clarification)*

Tree Disposition Plan

1. Updated Tree Disposition Plan to include several trees omitted from original submittal. Calculations were updated, showing an increase in percentage of protected tree inches saved to 77%. *(Clarification)*
2. Expanded note 4 to clarify the requirements for Tree Care Plans. *(Heritage Tree Foundation Request)*
3. Expanded note 5 to further restrict activities within the $\frac{3}{4}$ CRZ of Signature Grove Trees. *(Heritage Tree Foundation Request)*
4. Tree 3082 has been removed from the Tree Disposition Plan and Table because it was an 18" tree and does not meet the definition of a Protected or Heritage Tree. *(Clarification)*

Design Guidelines

1. The Development District Map (Figure 2.3 on page 7) was updated to reflect the new Jackson Avenue alignment. *(Clarification)*
2. Section 2.4.3 has been updated to reflect the correct park spaces and to allow "Park buildings and park structures" as permitted uses within the parks and open space. *(Clarification)*
3. The PUD Land Use Plan (Figures 2.5 and 2.5.1 on pages 9 and 10) has been updated with the new sheets. *(Clarification)*
4. The Master Framework Plan (Figure 3.3 on page 13) has been updated to reflect the new Jackson Avenue alignment. *(Clarification)*
5. The Bull Creek Road section (Figure 3.4.1 on page 14) has been updated to include a northbound bike lane. *(ZAP Recommendation)*
6. The Retail Main Street section (Figure 3.4.2a on page 15) has been updated to allow for alternative parking configurations. *(Per meeting with City Staff on 8/9/16)*
7. An alternative Retail Main Street section (Figure 3.4.2b on page 16) has been added to allow for a potential parkway configuration. *(Per meeting with City Staff on 8/9/16)*
8. An additional street section (Figure 3.5.8 on page 25) has been added to allow for a slip road. *(Per meeting with City Staff on 8/9/16)*
9. Section 3.6.2 regarding Typical Traffic Circles has been updated to clarify language and note that this is an optional facility. *(Clarification)*

10. Sections 4.3.1 (g), (h), and (i) have been updated to allow the slip road. *(Per meeting with City Staff on 8/9/16)*
11. Section 4.3.2(b)(4) has been added to allow for tandem parking. *(clarification)*
12. Section 4.3.2(c)(5) has been clarified that the front setback for front-facing garages is 5'. *(Clarification)*
13. Section 4.4.1(a) has been clarified regarding the requirement for pedestrian building entries. *(Clarification)*
14. Section 4.4.2(e)(5) has been added to clarify that glazing requirements do not apply to parking structures. *(Clarification)*
15. Section 4.4.3(c) and Figure 4.4.3 have been added to govern residential development adjacent to parks. *(Condition of PARD Superiority)*
16. Section 5.2.1 has been updated to reflect the correct park spaces. *(Condition of PARD Superiority)*
17. Section 5.2.4 has been revised to refer to the 1.25 acre park on Bull Creek Road as the "Pocket Park" rather than the "Neighborhood Park". *(PARD Request)*
18. The Framework Plan on page 36 of the March 2016 document has been removed because it was redundant. *(Clarification)*
19. The PUD Parks Plan (Figure 5.1 on page 39) has been added to the document. *(Clarification)*
20. Section 5.5.1 regarding Community Identity Signs has been updated to clarify intent. *(Clarification)*
21. Typos have been corrected throughout the document that do not change the language or intent of the text. *(Clarification)*

Master Plan

1. For reference, we are including a more current iteration of the Conceptual Master Plan.

The Grove at Shoal Creek PUD

Project Site Development Cumulation Table

December 22, 2015

This document is meant for tracking purposes to provide a cumulative tabulation for site development standards for the entire Planned Unit Development boundary. Individual Tracts and/or site plans may vary above or below the listed limits in the Site Development Regulations found on the Land Use Plan, as long as the calculation for the overall 75.76 acre PUD boundary does not exceed the limit. The applicant is responsible for keeping track of the amounts allotted and remaining in the "bucket" with each application.

The Grove at Shoal Creek shall not exceed the following based on gross site area of 75.76 acres:

Impervious Cover 65% = 2,145,069 S.F.

Dwelling Units 1,515 Units

Building Coverage 55% = 1,815,058 S.F.

<u>Case Number</u>	<u>Site Acreage</u>	<u>Impervious Cover</u>	<u>Dwelling Units</u>	<u>Building Coverage</u>
<u>TOTAL</u>	Acres	S.F.	UNITS	S.F.
REMAINING	75.76 Acres	2,145,069 S.F.	1,515 UNITS	1,815,058 S.F.

The Grove at Shoal Creek PUD**Tract Site Development Cumulation Table**

December 22, 2015

This document is meant for tracking purposes to provide a cumulative tabulation for site development standards for individual Tracts within the Planned Unit Development boundary. Individual site plans may vary above or below the listed limits in the Site Development Regulations found on the Land Use Plan, as long as the calculation for the overall Tract acreage boundary does not exceed the limit. The applicant is responsible for keeping track of the amounts allotted and remaining in the "bucket" with each application.

Tract A shall not exceed the following based on the gross site area of 7.02 acres:

Impervious Cover 55% = 168,185 S.F.
Dwelling Units 87 Units
Building Coverage 45% = 137,606 S.F.
Floor Area Ratio 0.75 :1

Case Number	Site Acreage	Impervious Cover	Dwelling Units	Building Coverage	Floor Area Ratio
TOTAL	Acres	S.F.	UNITS	S.F.	:1
REMAINING	7.02 Acres	168,185 S.F.	87 UNITS	137,606 S.F.	0.75 :1

Tract E shall not exceed the following based on the gross site area of 6.17 acres:

Impervious Cover 55% = 147,821 S.F.
Dwelling Units 77 Units
Building Coverage 45% = 120,944 S.F.
Floor Area Ratio 0.75 :1

Case Number	Site Plan Size	Impervious Cover	Dwelling Units	Building Coverage	Floor Area Ratio
TOTAL	Acres	S.F.	UNITS	S.F.	:1
REMAINING	6.17 Acres	147,821 S.F.	77 UNITS	120,944 S.F.	0.75 :1

Tract B shall not exceed the following based on the gross site area of 33.77 acres:

Floor Area Ratio 1.50 :1

Case Number	Site Plan Size	Floor Area Ratio
TOTAL	Acres	:1
REMAINING	33.77 Acres	1.50 :1

Tract C shall not exceed the following based on the gross site area of 10.62 acres:

Floor Area Ratio 0.75 :1

Case Number	Site Plan Size	Floor Area Ratio
TOTAL	Acres	:1
REMAINING	10.62 Acres	0.75 :1

Tract D shall not exceed the following based on the gross site area of 10.23 acres:

Floor Area Ratio 1.0 :1

Case Number	Site Plan Size	Floor Area Ratio
TOTAL	Acres	:1
REMAINING	10.23 Acres	1.0 :1

Tract F shall not exceed the following based on the gross site area of 3.42 acres:

Floor Area Ratio 1.0 :1

Case Number	Site Plan Size	Floor Area Ratio
TOTAL	Acres	:1
REMAINING	3.42 Acres	1.0 :1

Tract G shall not exceed the following based on the gross site area of 4.33 acres:

Floor Area Ratio 1.0 :1

Case Number	Site Plan Size	Floor Area Ratio
TOTAL	Acres	:1
REMAINING	4.33 Acres	1.0 :1

To: Garrett Martin, Milestone Community Builders & Caitlin Admire, Norris Design
From: Louis Alcorn, Capital Metro Planning
Date: 9 November 2015
Subject: RESPONSE - Site Evaluation for Potential Future Bus Stop Placement (*The Grove at Shoal Creek*)

Mr. Martin and Ms. Admire:

This is a follow up to our conversation regarding your project proposed along Bull Creek Road south of West 45th Street, *The Grove at Shoal Creek*. As you know Capital Metro currently operates one bus route, bi-directionally along Bull Creek Road, with fully accessible bus stops already located within the vicinity of your project's extent. Currently, Capital Metro is conducting a holistic service plan update (*Connections 2025*) during which all routes will be evaluated in terms of productivity, efficiency, and equity with respect to Austin's rapidly changing urban environment in order to recommend service changes and/or expansions to be implemented in the next five to ten years. The 19-Bull Creek route will be evaluated in this process and staff remains optimistic that transit-supportive densities in the form of new development should lead to increased ridership along this corridor.

Regarding your question concerning the flexibility of moving or redesigning certain bus stops to better serve future residents, we would be interested in working with you to identify suitable areas for new and improved bus stops. Our current stops within proximity to the project extent (identified on the attached map) exist in the following locations:

- Northbound (NB): Along Bull Creek Rd. at West 45th St., Jackson Ave., and West 39th Street
- Southbound (SB): Along Bull Creek Rd. at West 44th St., Jackson Ave., and West 40th Street

As per the Fall 2015 Update to our Service Guidelines and Standards, stop spacing in an area with medium density should generally be a minimum of 1,200 feet. This being said, the following map depicts our proposed bus stop placements, including the potential to relocate the W 45th & Bull Creek NB stop up to 300 feet south of its current location.

Bus stops should be approximately 25 feet in length by 10 feet in width and incorporated into the sidewalk. A larger area of approximately 15 feet in width (perpendicular to the road by 50 feet in length (parallel to the road) surrounding the stop should maintain a level slope to ensure that all ADA slope requirements are met.

What I have suggested here is based on what we know today and what is depicted in your site plan. We would want to continue to work with you as you develop your plans to help in refining these suggestions as you move forward.

Louis Alcorn
Capital Metro – Planning
(512) 389-7491

June 26, 2015

To: Jerry Rusthoven, COA Department of Planning and Zoning

From: Shoalmont Property Owners (includes all houses on the following streets which are owner-occupied)

2600-2607 LaRonde

4500-4713 Chiappero

4500-4707 Oakmont

4500-4807 Finley

2600-2615 W. 48th

Neighborhood Contact: Carolyn Mixon

4616 Chiappero Trl

Austin, TX 78731

 512-423-0650

Re: Milestone Community Builders Proposed Development "The Grove at Shoal Creek"

COA Case # CD-2015-0009

PUD Application # C814-2015-0074

We are sending you a copy of our neighborhood letter outlining our concerns and priorities regarding the proposed Milestone project at 45th and Bull Creek. We would greatly appreciate it if you would take these into consideration as you make decisions regarding the development plans submitted by Milestone. We are not opposed to the development of the property, but the scope of the development is too large for the streets serving it and does not fit with the character of the surrounding neighborhoods.

Of 107 owner-occupied houses on the surveyed streets, owners (1-2) of 50 houses have signed this letter in support of greatly reducing the density of Milestone's project and increasing greenspace and buffer zone. Prior to signing, all owners received information about the development compared to other similar central Austin developments and copy of letter. Most of those who wished to sign contacted me (Carolyn Mixon) while others were obtained by knocking on doors. Due to time constraints, approximately 57 doors were not approached for signing, and it cannot be assumed that they are not in favor of the letter's content. On the contrary, those approached who had not previously contacted me were overwhelmingly in favor of a reduction of this project.

Thank you for your time and consideration.

June 26, 2015

To: Sherri Sirwaitis, COA Department of Planning and Zoning

From: Shoalmont Property Owners (includes all houses on the following streets which are owner-occupied)

2600-2607 LaRonde

4500-4713 Chiappero

4500-4707 Oakmont

4500-4807 Finley

2600-2615 W. 48th

Neighborhood Contact: Carolyn Mixon

4616 Chiappero Trl

Austin, TX 78731

pcmom54@yahoo.com 512-423-0650

Re: Milestone Community Builders Proposed Development "The Grove at Shoal Creek"

COA Case # CD-2015-0009

PUD Application # C814-2015-0074

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Thank you for your time and consideration.

June 26, 2015

ATTN: Garrett Martin, President and CEO
Milestone Community Builders, LLC
ARG Bull Creek, LTD
9111 Jollyville Road, Suite 111
Austin, Texas 78759

RE: Milestone Community Builders Proposed Development "The Grove at Shoal Creek"
COA Case Number CD-2015-0009, PUD Application # C814-2015-0074

Mr. Martin,

As homeowners in the Shoalmont neighborhood and specifically as homeowners on Chiappero, W. 48th, Oakmont, Finley, and La Ronde, we are contacting you about our priorities regarding Milestone's proposed multi-use development "The Grove at Shoal Creek". Many of us have attended your public meetings and provided input via your surveys. We appreciated your attendance at our neighborhood meeting at NW Recreation Center but would like to more clearly outline our concerns and priorities as residents of the aforementioned streets which connect to W. 45th.

We have similar concerns and priorities as the W. 45th St. homeowners who have been in communication with you, and we would appreciate your careful consideration of these:

1. While your conclusion from the collected surveys is that 67% of residents prefer high-density and more open space, we oppose your plan to put 150,000 SF of retail (more than 1.5x Mueller), 225,000 SF of offices (roughly equivalent to Mueller), and 1010 apartments/condos (similar to Triangle) in addition to a hotel, hospital, and single-family housing in the middle of our neighborhoods. Mueller and the Triangle are not surrounded by single-family neighborhoods immediately adjacent to the properties (with the exception of Delwood) as is The Grove. They are served by major, multi-lane arterial streets which have long handled commercial, delivery and office traffic. Your proposal to widen the 45th/Bull Creek intersection at the expense of trees and residents' yards/homes will do little to help the extraordinary amount of car and delivery truck traffic that your project will generate both day and night on our residential part of W. 45th between Burnet Rd. and Bull Creek. We believe that the proposed office and retail density should be cut at least in half.

2. Regarding open space, it is clear that you believe the 12-acre signature park together with plazas and curbside beds is more than adequate compensation for the commercialization of our neighborhoods and the traffic problems that we will experience. We strongly support a greenbelt buffer of similar scope as that of the Mueller Greenways which separate the Delwood neighborhood from the development. This would also make the open space more accessible to our neighborhood as the currently proposed "signature" park is buried behind the whole project. This is not unprecedented or unacceptable in the urban core as the Mueller Greenways are a prime example of how quiet, long-time neighborhoods can be buffered from a larger, high-density development with positive effects for all.

3. We strongly oppose any access other than walking/biking to The Grove from midpoints on W. 45th and in particular, a street through the 2627 W. 45th property that your company has purchased for that purpose. This property would best be suited for pedestrian and bicycle connectivity between the previously-described greenway, Rosedale and our neighborhood to the north. Any car access/exit at midpoints of W.45th between the Shoal Creek bridge and Bull Creek together with the increased volume of traffic that your development will produce will exacerbate the already dangerous situation that we have at peak rush hours in attempting to exit our neighborhood onto W. 45th and entering our streets from W. 45th.

4. We understand from your presentations that you envision a restaurant or other commercial establishment at the corner of Bull Creek and W. 45th. We would urge you to reconsider this plan as it will only increase the traffic congestion at the intersection and increase noise levels for neighbors. Again, the previously discussed greenway buffer would be a better choice for this section as it would also provide neighborhood access to the open space without having to navigate through the proposed residential and commercial development.

In the public meetings, you have spoken frequently of the need for compromise. We believe that your company could compromise with us to make our priorities a reality while still realizing an economically-viable project for your company. We would appreciate your genuine consideration of our priorities and look forward to constructive communication. We would like to request a meeting with you in the near future to discuss these topics further.

Sincerely,

The Undersigned Shoalmont Property Owners of Chiappero, W. 48th, Oakmont, Finley, and La Ronde

Shoalmont Property Owners (includes all houses on the following streets which are owner-occupied)

2600-2607 LaRonde
4500-4713 Chiappero
4500-4707 Oakmont
4500-4807 Finley
2600-2615 W. 48th

Neighborhood Contact: Carolyn Mixon

4616 Chiappero Trl

Austin, TX 78731

pcmom54@yahoo.com 512-423-0650

NOTE:

Of 107 owner-occupied houses on the surveyed streets, owners (1-2) of 50 houses have signed this letter in support of greatly reducing the density of Milestone's project and increasing greenspace and buffer zone. Prior to signing, all owners received information about the development compared to other similar central Austin developments and copy of letter. Most of those who wished to sign contacted me (Carolyn Mixon) while others were obtained by knocking on doors. Due to time

constraints, approximately 57 doors were not approached for signing, and it cannot be assumed that they are not in favor of the letter's content. On the contrary, those approached who had not previously contacted me were overwhelmingly in favor of a reduction of this project.

cc: Sheri Gallo, Council Member, City of Austin District 10
Leslie Pool, Council Member, City of Austin District 7
Steve Adler, Mayor, City of Austin
Kathie Tovo, Mayor Pro-tem and Council Member, City of Austin District 9
Ora Houston, Council Member, City of Austin District 1
Delia Garza, Council Member, City of Austin District 2
Sabino "Pio" Renteria, Council Member, City of Austin District 3
Gregorio Casar, Council Member, City of Austin District 4
Ann Kitchen, Council Member, City of Austin District 5
Don Zimmerman, Council Member, City of Austin District 6
Ellen Troxclair, Council Member, City of Austin District 8
Sherri Sirwaitis, Case Manager, City of Austin Department of Planning and Zoning
Jerry Rusthoven, Case Manager, City of Austin Department of Planning and Zoning
Kathleen Fox, City of Austin Comprehensive Planning
Marilyn Shashoua, City of Austin PARD Planning and Design Review
Bryan Golden, City of Austin Transportation
Bull Creek Road Coalition (via listserv email distribution)

June 26, 2015

ATTN: Garrett Martin, President and CEO

Milestone Community Builders, LLC

ARG Bull Creek, LTD

9111 Jollyville Road, Suite 111

Austin, Texas 78759


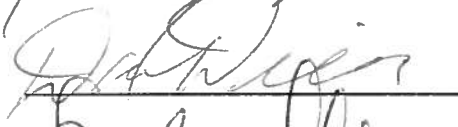
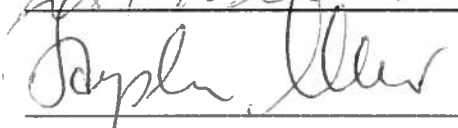
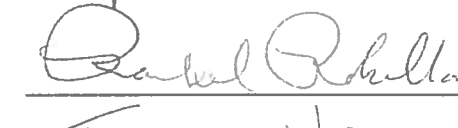
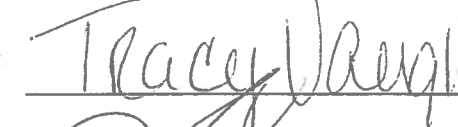

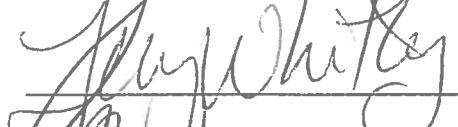
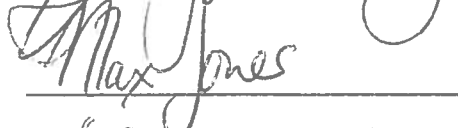
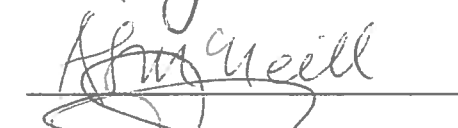

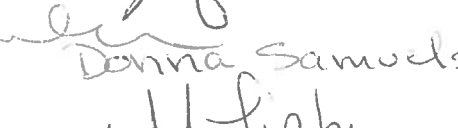

RE: Milestone Community Builders Proposed Development "The Grove at Shoal Creek"

COA Case Number CD-2015-0009

PuD # C 814-2015-0074

Sincerely,

The Undersigned Shoalmont Property Owners of Chiappero Trl and W. 48th, 78731

PRINT NAME	SIGNATURE	ADDRESS
Jan Triplett		4605 Chiappero Tr
Dor Densen		4605 Chiappero Tr
STEPHAN MILVIES		4702 - 4 -
R. Robillard		4703
Tracey Vaught		4707 Chiappero Tr
Jimmy Vaught		//
Tracey Whitley		4502 Chiappero Trail
Max Jones		4502 Chiappero Trail
A.S. McNEILL		4504 CHIAPPERO TR 4506 - rental owned by 45c
Scott A. Samuelson		4615 Chiappero Tr
Donna Samuelson		" " "
MIHAI SIRBU		4711 CHIAPPERO TRL

June 26, 2015

ATTN: Garrett Martin, President and CEO
Milestone Community Builders, LLC
ARG Bull Creek, LTD
9111 Jollyville Road, Suite 111
Austin, Texas 78759

RE: Milestone Community Builders Proposed Development "The Grove at Shoal Creek"
COA Case Number CD-2015-0009
PUD # C 814-2015-0074

Sincerely,

The Undersigned Shoalmont Property Owners of Chiappero Trl and W. 48th, 78731

PRINT NAME	SIGNATURE	ADDRESS
<u>Carolyn Nixon</u>	<u>Carolyn Nixon</u>	<u>4616 Chiappero Trl.</u>
<u>Eddie Nixon</u>	<u>Eddie Nixon</u>	<u>4616 Chiappero Trl</u>
<u>Susan Tarrett</u>	<u>Susan Tarrett</u>	<u>4701 Chiappero Trl.</u>
<u>Mary Holman</u>	<u>Mary Holman</u>	<u>2604 West 48th St</u>
<u>Jim Lyons</u>	<u>Jim Lyons</u>	<u>2604 W 48th St</u>
<u>Julia O. Willhoite</u>	<u>Julia O. Willhoite</u>	<u>4601 Chiappero ^{Trail}</u>
<u>Mary Blackstock</u>	<u>Mary Blackstock</u>	<u>2607 La Ronde</u>
<u>Todd Thompson</u>	<u>Todd Thompson</u>	<u>2602 La Ronde St.</u>
<u>Ashley Thompson</u>	<u>Ashley Thompson</u>	<u>2602 La Ronde St</u>
<u>Inga Werstreet</u>	<u>Inga Werstreet</u>	<u>2603 La Ronde St</u>

June 26, 2015

ATTN: Garrett Martin, President and CEO
Milestone Community Builders, LLC
ARG Bull Creek, LTD
9111 Jollyville Road, Suite 111
Austin, Texas 78759

RE: Milestone Community Builders Proposed Development "The Grove at Shoal Creek"
COA Case Number CD-2015-0009
PUD # C814-2015-0074

Sincerely,

The Undersigned Shoalmont Property Owners of La Ronde, 78731

PRINT NAME

SIGNATURE

ADDRESS

MARY ROBBIN'S

Mary Robbins

2600 La Ronde St.

Carl Brockman

Carl Brockman

2600 La Ronde St

John Ribble

John Ribble

2601 La Ronde

STEPHEN SCHOTTMAN

Step Schottman

2605 LA RONDE

COA Case Number CD-2015-0009

PUD # C814-2015-0074

Sincerely,

The Undersigned Shoalmont Property Owners of Chiappero Trl and W. 48th, 78731

PRINT NAME

SIGNATURE

ADDRESS

JAIRAM KALYANA-
-SUNDARAM

Edison

4508 CHIAPPERO
TRAIL, AUSTIN, TX
78731

NANDINI JAIRAM

Nandini Jairam

4508 CHIAPPERO TRL
AUSTIN, TX 78731

June 26, 2015

ATTN: Garrett Martin, President and CEO
Milestone Community Builders, LLC
ARG Bull Creek, LTD
9111 Jollyville Road, Suite 111

June 26, 2015

ATTN: Garrett Martin, President and CEO
Milestone Community Builders, LLC
ARG Bull Creek, LTD
9111 Jollyville Road, Suite 111
Austin, Texas 78759

RE: Milestone Community Builders Proposed Development "The Grove at Shoal Creek"
COA Case Number CD-2015-0009
PUD #C814-2015-0074

Sincerely,

The Undersigned Shoalmont Property Owners of **Chiappero Trl and W. 48th, 78731**

PRINT NAME	SIGNATURE	ADDRESS
<u>DAVID MORRIS</u>	<u>[Signature]</u>	<u>2606 W 48TH AUSTIN TX 78731</u>
<u>Latha Joyce</u>	<u>[Signature]</u>	<u>2612 W 48th St. 78731</u>
<u>Don Joyce</u>	<u>[Signature]</u>	<u>2612 W. 48th St. 78731</u>
<u>Cara Mueller</u>	<u>[Signature]</u>	<u>2615 W. 48th St 78731</u>
<u>Raúl Madrid</u>	<u>[Signature]</u>	<u>4704 Chiappero Tr. 78731</u>

In the public meetings, you have spoken frequently of the need for compromise. We believe that your company could compromise with us to make our priorities a reality while still realizing an economically-viable project for your company. We would appreciate your genuine consideration of our priorities and look forward to constructive communication. We would like to request a meeting with you in the near future to discuss these topics further.

Sincerely,

The Undersigned Shoalmont Property Owners of Chiappero, Oakmont, Finley, and La Ronde

2601 W. 48th
78731

[Signature] Jeff Rowles
Anne Vargas Pra
Anne Vargas Pra