

## Welcome!

### Please sign in

At this open house, you will have the opportunity to:

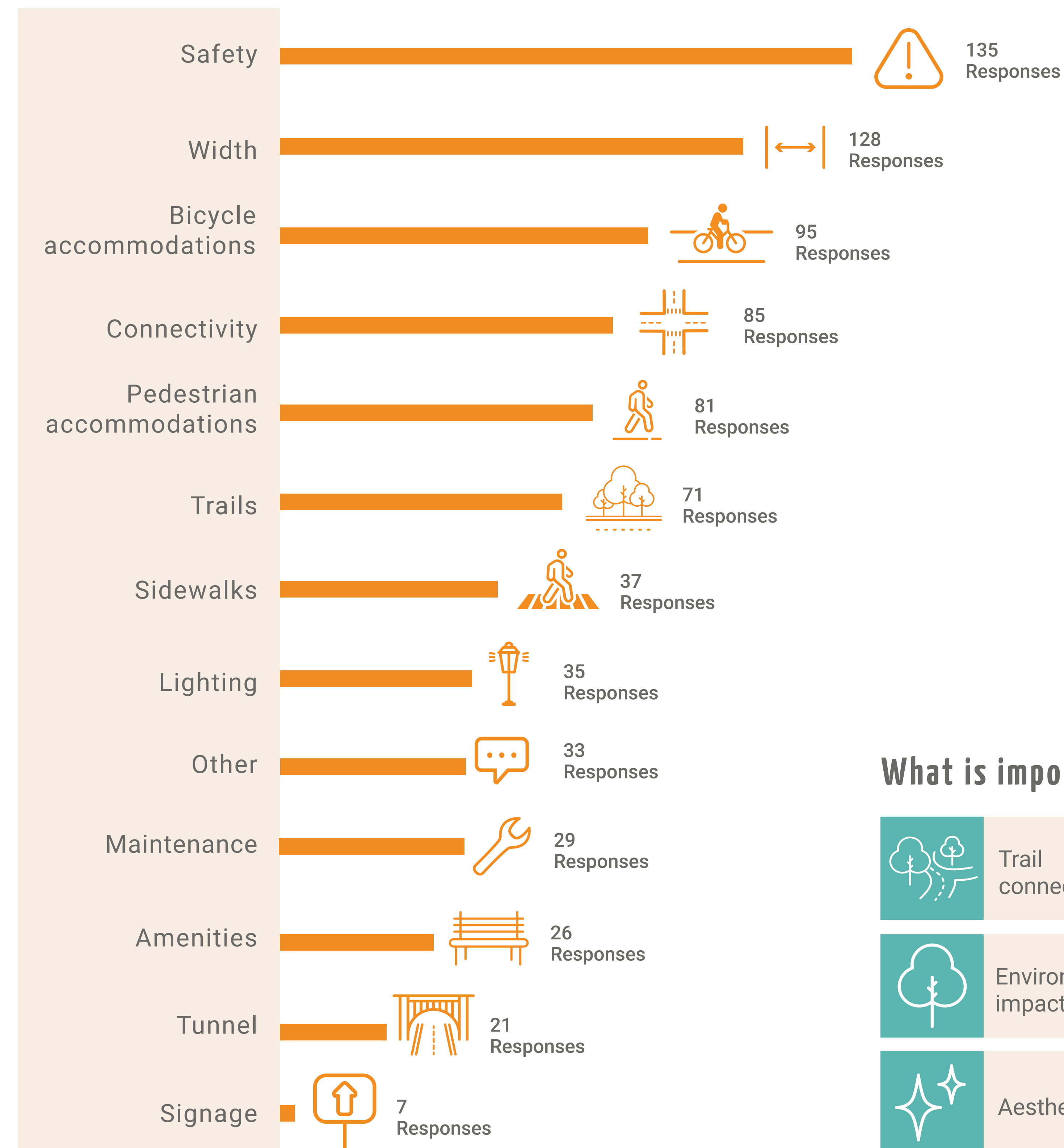
- Review previous public input on this project
- Review draft alternatives
- Review proposed interim improvements
- Take our survey
- Discuss the project with staff

# PUBLIC INPUT

In 2018, the City of Austin began work on a Preliminary Engineering Report to evaluate the possibility of a new pedestrian and bicycle bridge over Lady Bird Lake, as well as a review of possible interim improvements to the existing bridge.

This report summarizes input received in person at the public meeting and through the survey. Over 550 responses were received and analyzed. Input received from the community is being evaluated to develop alternatives for public review.

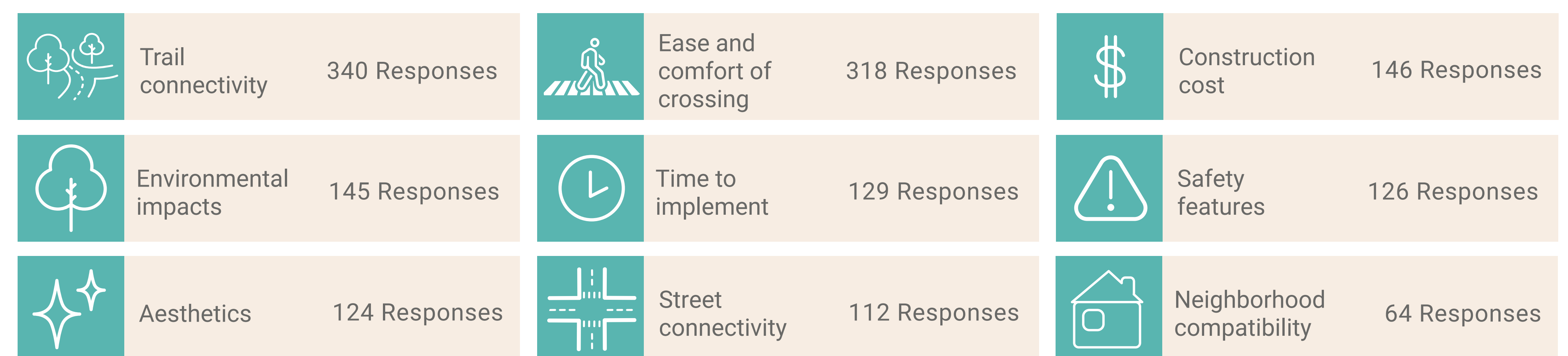
## What needs improvement?



## What do you like?



## What is important? (Evaluation criteria)

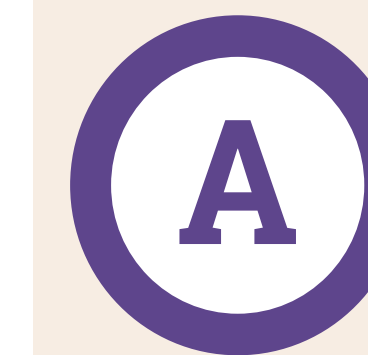




# PUBLIC INPUT

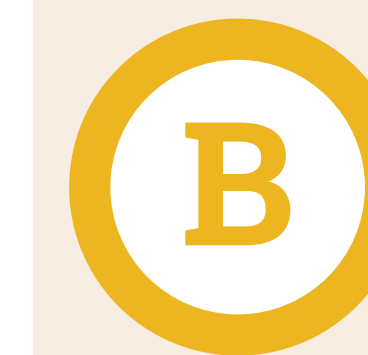
## What is your ideal bridge alignment?

Open house participants and online survey respondents reported using the area, including the bridge, connecting trails, and local streets, for riding bikes, walking, and running on the Butler Trail Loop. Participants indicated they use the existing infrastructure as a north-south connection from S. Pleasant Valley Rd. to Cesar Chavez St. and from the Butler Trail to Pedernales St. Participants also indicated they use the area as an east-west connection from Lakeshore Blvd to the Country Club Creek Trail, and as a connection to the Guerrero Park Trail.



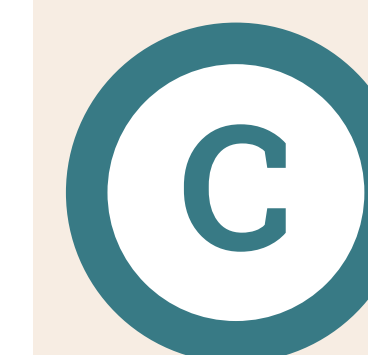
Upstream from the dam,  
connecting to the  
peninsula

156  
Responses



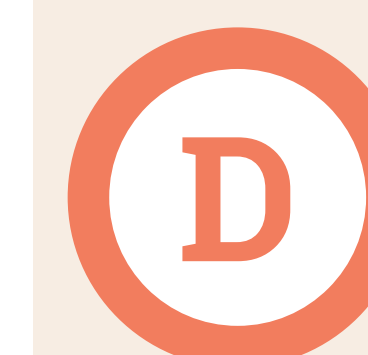
Wishbone shaped bridge  
that connects to the shore at  
three points\*

13  
Responses



Upstream from the dam,  
parallel to Pleasant  
Valley Road

176  
Responses



Downstream from the dam,  
parallel to Pleasant Valley  
Road and connected to Ann  
and Roy Butler Bike Trail

63  
Responses



Widening the existing  
bridge

67  
Responses

No build

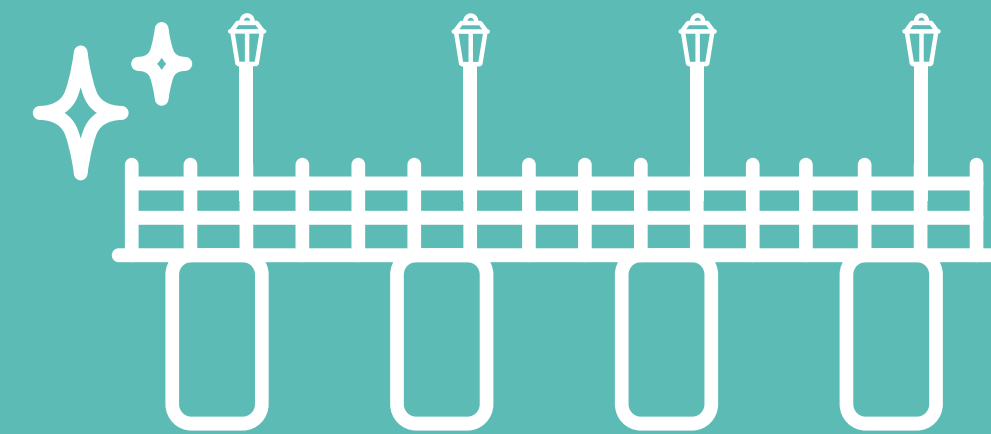
9  
Responses

\*Alignments A, C, D + E were presented as options on the online survey; alignment B was drawn by attendees at the open house

# PUBLIC INPUT

## Which bridge type do you prefer?

Hybrid Conventional Bridge



\$7.1 to \$8.4 million

215 Responses

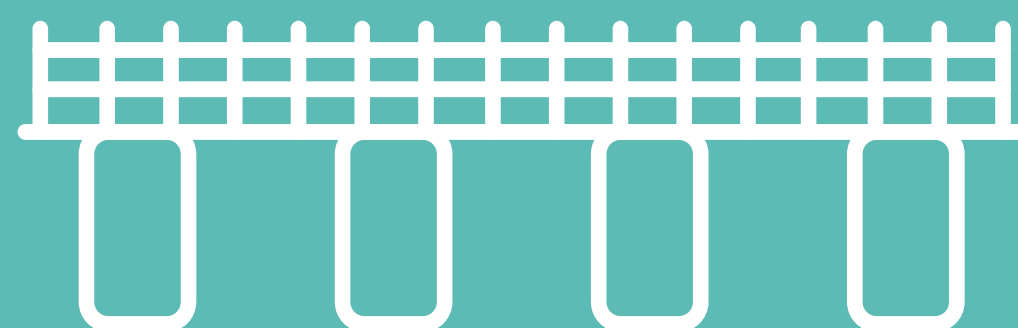
Truss Bridge



\$7.0 to \$8.3 million

116 Responses

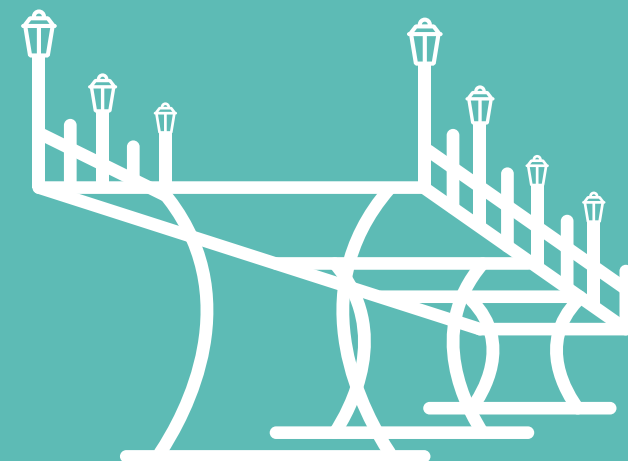
Conventional Bridge



\$6.7 to \$8.0 million

94 Responses

Custom Beam Bridge



\$10.4 to 12.4 million

46 Responses

Iconic Bridge



\$21.7 to 26.2 million

34 Responses

## What improvements should happen sooner?

Increase sidewalk widths on bridge

387 Responses

Improve north and south pedestrian and bicycle connections to bridge

352 Responses

Improve pedestrian crossings on Pleasant Valley Road

229 Responses

Reduce speed on Pleasant Valley Road

118 Responses

Make pedestrian bridge ADA compliant

111 Responses

Remove chain link fence on bridge

101 Responses

Upgrade bridge handrails

95 Responses



# ABOUT THE ALTERNATIVES

Based on feedback from the first public meeting and survey, five build alternatives and the no build were progressed.

**Alt. A** – “Peninsula Node” Alignment - Upstream

**Alt. B** – “Wishbone” Alignment - Upstream

**Alt. C** – “Arc” Alignment - Upstream

**Alt. D** – “Arc” Alignment - Downstream

**Alt. E** – “Widen Existing Bridge” Alignment

**No build**

## Common Elements of Build Alternatives

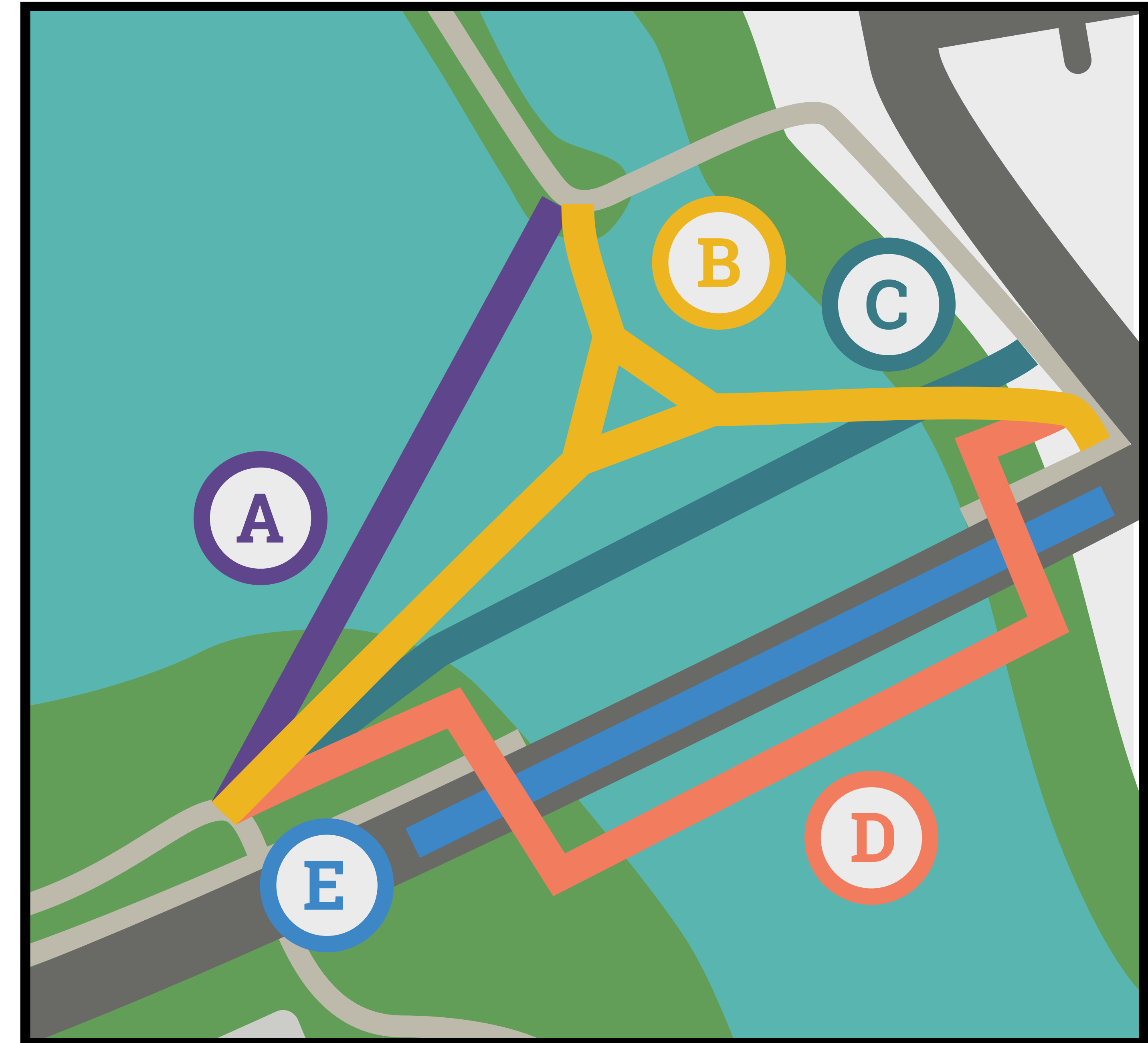
- All would widen the existing tunnel on south side of the bridge
- All would improve park land on north shore, including relocation of parking lot to street edge
- All include a shared use path connecting to the Cesar Chavez St. intersection
- All have similar levels of disturbed area

## Storm Water Analysis

- All alternatives have been analyzed and are compatible with 100-year storm flows (no rise in floodplain and structurally sound)
- All alternatives provide adequate access for dam maintenance

## Cost and Timeline

- Construction funding for a new bridge has not been identified
- As costs increase, the time needed to secure construction funding may increase





# ALTERNATIVE A: "PENINSULA NODE" ALIGNMENT - UPSTREAM

## ILLUSTRATIVE SITE PLAN

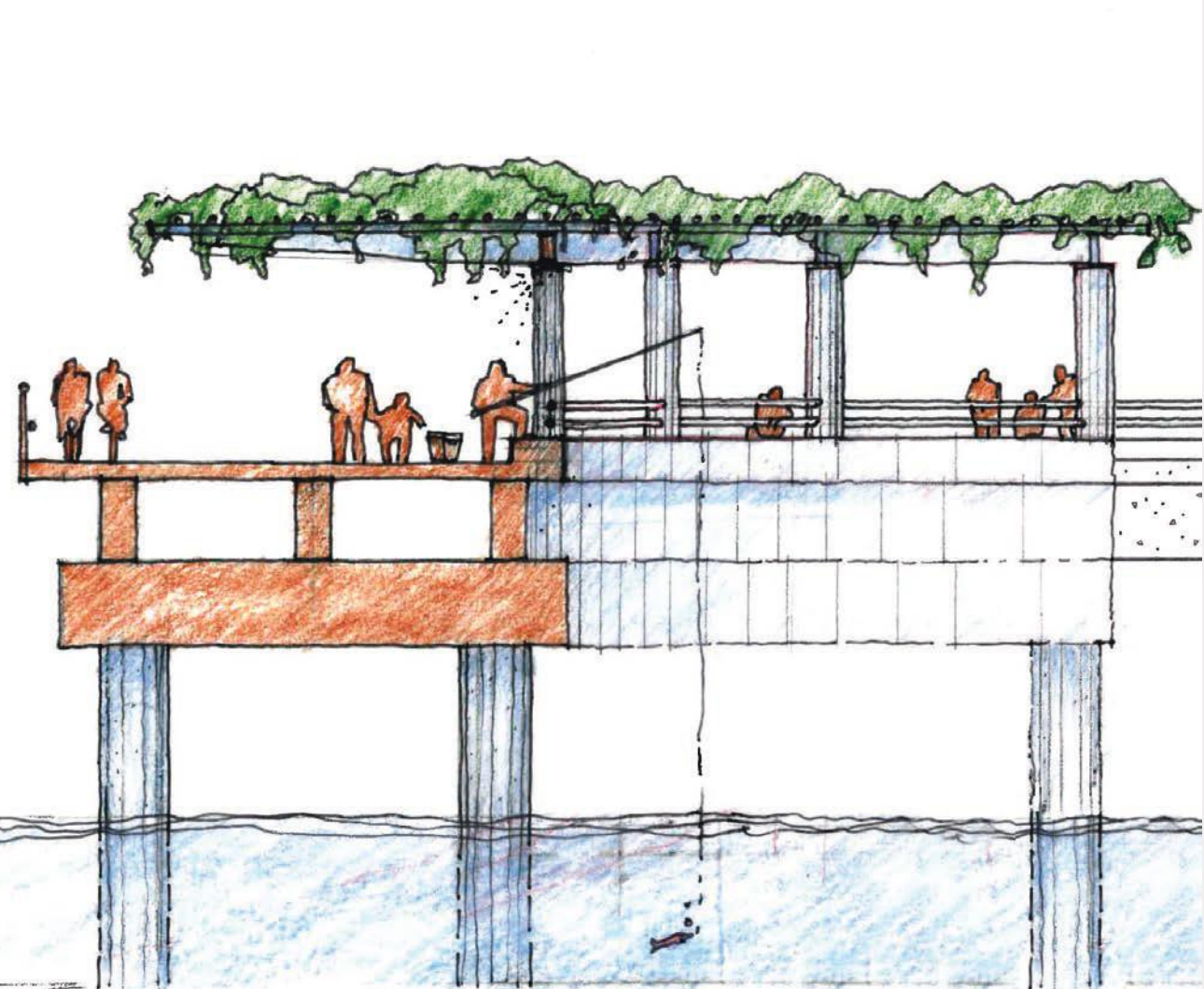
COST: ~\$12.9 MILLION



## PERSPECTIVE VIEW, LOOKING NORTHWEST



## CROSS-SECTION NEAR TIP OF PENINSULA



### FEATURES

- SEPARATED FROM ROADWAY TRAFFIC
- PRESERVES AND INTEGRATES EXISTING ATMOSPHERE OF THE PENINSULA
- PROVIDES UPSTREAM VIEWS OF LAKE AND DOWNSTREAM RIVER VIEWS THROUGH THE DAM STRUCTURE
- ALIGNMENT MINIMIZES ELEVATION CHANGE ALONG TRAIL

### TRADE-OFFS

- PROVIDES DIRECT CONNECTIONS TO BUTLER TRAIL, BUT TRAVEL ALONG PLEASANT VALLEY RD IS LESS DIRECT
- ALL ALTERNATIVES REQUIRE MODIFICATION TO THE PENINSULA TO EITHER TIE IN NEW BRIDGE OR TO NOT IMPACT 100-YEAR STORM FLOWS
- ALL ALTERNATIVES HAVE SIMILAR LEVELS OF DISTURBED AREA



# ALTERNATIVE B: "WISHBONE" ALIGNMENT - UPSTREAM

## ILLUSTRATIVE SITE PLAN

COST: ~\$13.5 MILLION



## PERSPECTIVE VIEW, LOOKING NORTH



## CROSS-SECTION @ TRAIL INTERSECTION ABOVE WATER



### FEATURES

- SEPARATED FROM ROADWAY TRAFFIC
- PROVIDES DIRECT CONNECTIONS TO PLEASANT VALLEY AND BUTLER TRAIL IN ONE STRUCTURE GIVING CHOICE OF TRAVEL PATHS
- PROVIDES WIDE, UPSTREAM VIEWS OF LAKE AND DOWNSTREAM RIVER VIEWS THROUGH THE DAM STRUCTURE
- ALIGNMENT MINIMIZES ELEVATION CHANGE ALONG TRAIL
- OFFERS LARGE PUBLIC SPACE AREA ON BRIDGE

### TRADE-OFFS

- ALL ALTERNATIVES REQUIRE MODIFICATION TO THE PENINSULA TO EITHER TIE IN NEW BRIDGE OR TO NOT IMPACT 100-YEAR STORM FLOWS
- ALL ALTERNATIVES HAVE SIMILAR LEVELS OF DISTURBED AREA



# ALTERNATIVE C: "ARC" ALIGNMENT - UPSTREAM

## ILLUSTRATIVE SITE PLAN

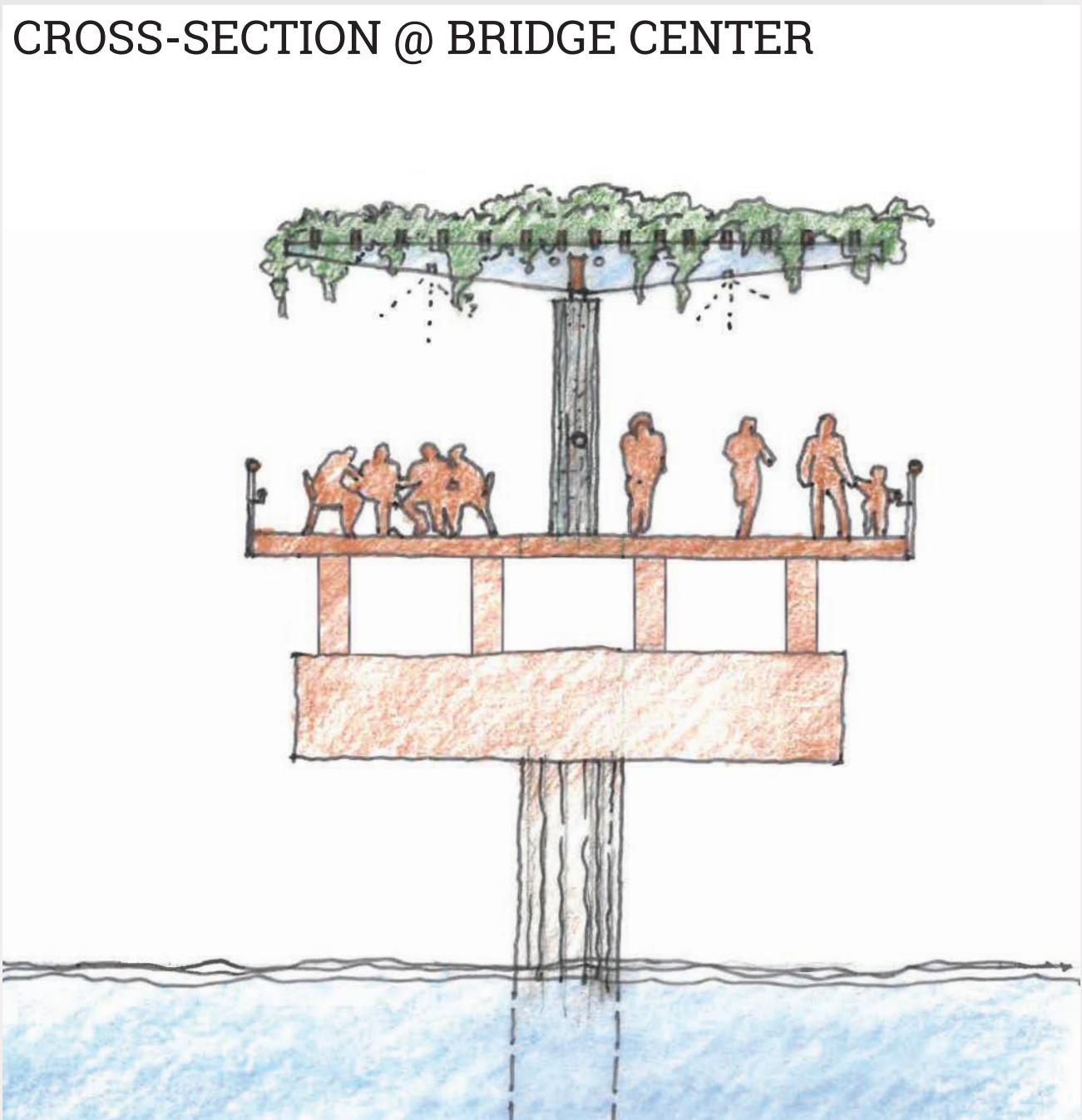
COST: ~\$10.6 MILLION



## PERSPECTIVE VIEW



## CROSS-SECTION @ BRIDGE CENTER



## FEATURES

- SEPARATED FROM ROADWAY TRAFFIC
- PROVIDES WIDE, UPSTREAM VIEWS OF LAKE AND DOWNSTREAM RIVER VIEWS THROUGH THE DAM STRUCTURE
- THE IMPROVED TRAIL PASSES BY THE ROY MONTELONGO SCENIC OVERLOOK, ADDING PROMINENCE TO THIS EXISTING PUBLIC ART INSTALLATION THIS EXISTING TRAIL COULD BE WIDENED OR REROUTED.

## TRADE-OFFS

- PROVIDES DIRECT CONNECTIONS TO PLEASANT VALLEY RD, BUT BUTLER TRAIL REQUIRES AN ADDITIONAL BRIDGE CROSSING AND ELEVATION CHANGE
- ALL ALTERNATIVES REQUIRE MODIFICATION TO THE PENINSULA TO EITHER TIE IN NEW BRIDGE OR TO NOT IMPACT 100-YEAR STORM FLOWS
- ALL ALTERNATIVES HAVE SIMILAR LEVELS OF DISTURBED AREA



# ALTERNATIVE D: "ARC" ALIGNMENT - DOWNSTREAM

## ILLUSTRATIVE SITE PLAN

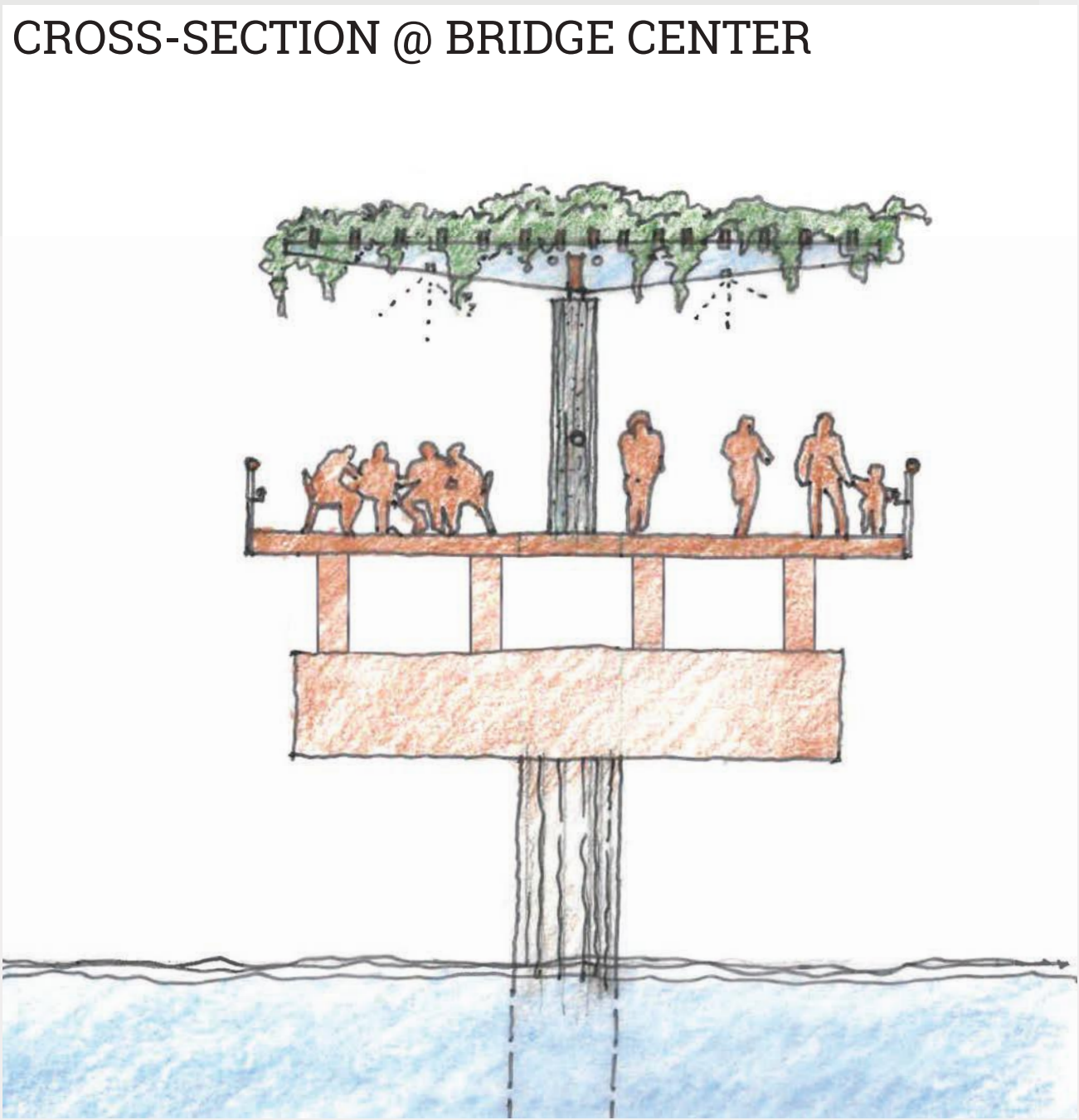
COST: ~\$14.5 MILLION



## PERSPECTIVE VIEW, LOOKING SOUTH



## CROSS-SECTION @ BRIDGE CENTER



## FEATURES

- SEPARATED FROM ROADWAY TRAFFIC
- PROVIDES A RARE, DOWNSTREAM RIVER VIEW
- GRADE-SEPARATED CROSSINGS ON BOTH SIDES OF LADY BIRD LAKE ALLOW ALL FOUR QUADRANTS OF THE BRIDGE EASY ACCESS TO THE BRIDGE AND TRAIL NETWORK
- THE IMPROVED TRAIL PASSES BY THE ROY MONTELONGO SCENIC OVERLOOK, ADDING PROMINENCE TO THIS EXISTING PUBLIC ART INSTALLATION. THIS EXISTING TRAIL COULD BE WIDENED OR REROUTED.

## TRADE-OFFS

- PROVIDES DIRECT CONNECTIONS TO PLEASANT VALLEY RD, BUT BUTLER TRAIL REQUIRES AN ADDITIONAL BRIDGE CROSSING AND ELEVATION CHANGE.
- CONSTRUCTION WILL RESULT IN ADDITIONAL TRAFFIC IMPACTS ALONG PLEASANT VALLEY RD FOR THE UNDERCROSSING AT NORTH END OF BRIDGE.
- ALL ALTERNATIVES REQUIRE MODIFICATION TO THE PENINSULA TO EITHER TIE IN NEW BRIDGE OR TO NOT IMPACT 100-YEAR STORM FLOWS
- ALL ALTERNATIVES HAVE SIMILAR LEVELS OF DISTURBED AREA



# ALTERNATIVE E: “WIDEN EXISTING BRIDGE” ALIGNMENT - DOWNSTREAM

## ILLUSTRATIVE SITE PLAN

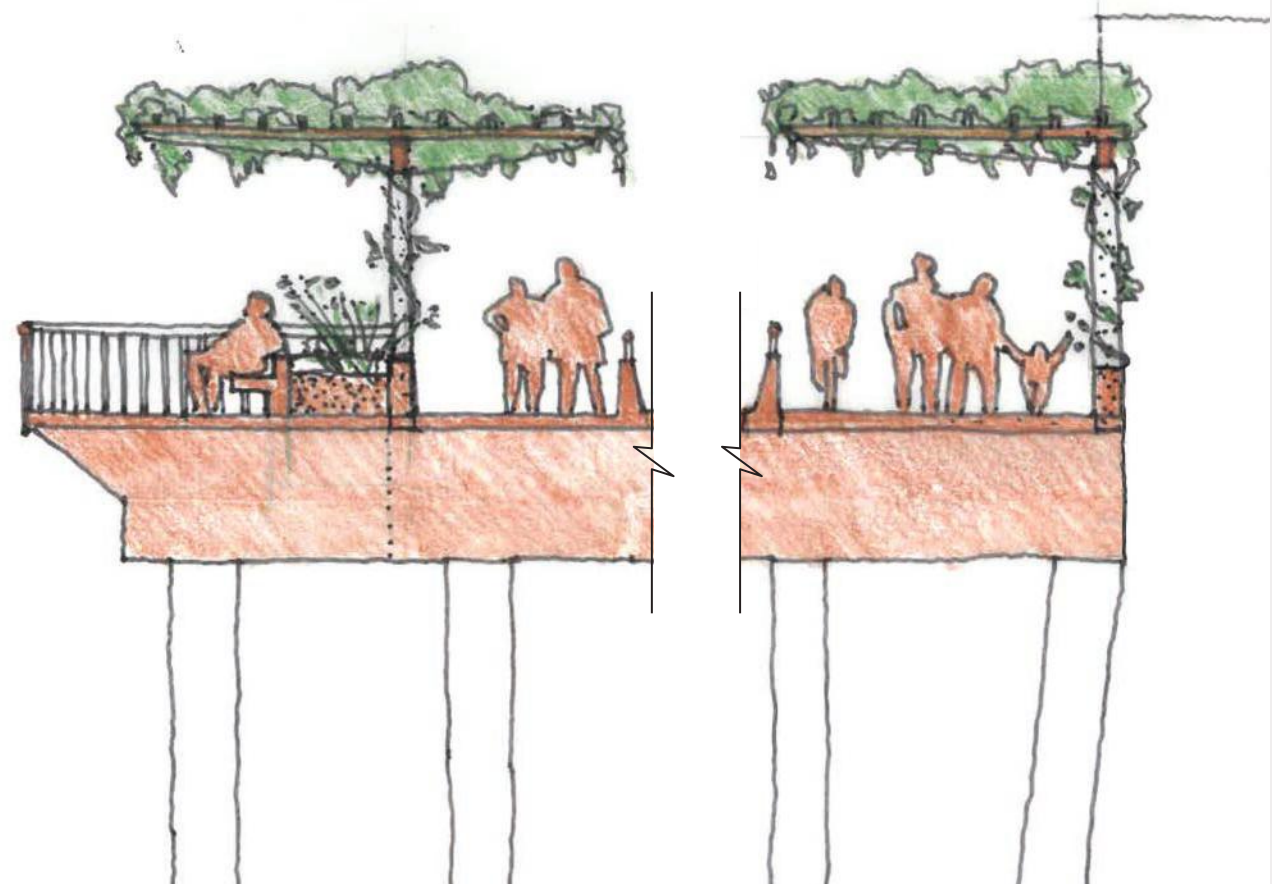
COST: ~\$12.0 MILLION



## PERSPECTIVE VIEW, LOOKING SOUTH



## CROSS-SECTION @ BRIDGE CENTER, LOOKING SOUTH



## FEATURES

- PROVIDES MOST DIRECT CONNECTIONS ALONG PLEASANT VALLEY RD
- PROVIDES UPSTREAM VIEWS OF LAKE ON THE WEST SIDE AND DOWNSTREAM RIVER VIEW ON THE EAST SIDE
- THE IMPROVED TRAIL PASSES BY THE ROY MONTELONGO SCENIC OVERLOOK, ADDING PROMINENCE TO THIS EXISTING PUBLIC ART INSTALLATION. THIS EXISTING TRAIL COULD BE WIDENED OR REROUTED

## TRADE-OFFS

- NOT SEPARATED FROM ROADWAY TRAFFIC
- REALIGNMENT OF ROADWAY SURFACE RESULTS IN ADDITIONAL TRAFFIC IMPACTS DURING CONSTRUCTION
- ALL ALTERNATIVES REQUIRE MODIFICATION TO THE PENINSULA TO EITHER TIE IN NEW BRIDGE OR TO NOT IMPACT 100-YEAR STORM FLOWS
- ALL ALTERNATIVES HAVE SIMILAR LEVELS OF DISTURBED AREA



# DRAFT GUIDING PRINCIPLES FOR BRIDGE DESIGN

The intent of the guiding principles is to establish a design framework for the project. The guiding principles have been drafted in response to public input and technical analysis. All alternatives should meet the standards of the guiding principles. We are seeking feedback on the draft guiding principles below and will refine this draft in response to feedback. Ultimately, the guiding principles will be carried forward and applied to the design phase of the project.

## ① SAFETY & ACCESSIBILITY:

Design the bridge and its connecting trails to high standards of safety and accessibility.

- Provide night sky-friendly lighting for safe and comfortable 24-hour use
- Provide special lighting at wider areas of the bridge integrated into shade structures
- Appropriate bridge width for high levels of use and opportunities to linger.
- The existing narrow tunnel providing pedestrian connection to Roy G. Guerrero Park and trails should be upgraded.



Lighting for the bridge, trail and public places is essential.



A new, spacious underpass will replace the existing pedestrian tunnel.

## ② CONVENIENT CONNECTIONS:

Provide a bridge alignment and connections that create direct and convenient travel paths for both commuters and recreational trail users.

- Trail slopes should be comfortable for all ages and abilities with a maximum grade of 5%.
- High-quality connections should be made to accommodate the significant travel demand, including at the Pleasant Valley Rd crossings and the Ann and Roy Butler Trail Loop.



The existing parking lot on parkland will be removed to allow for the bridge landing and an enhanced trailhead, and new on-street parking will be striped along Canterbury St.

## ③ ENVIRONMENT & LANDSCAPE:

Design the bridge to complement and enhance the environment, park, river, and the natural landscape.

- Bridge landings into nearby parkland should be thoughtfully designed and integrated into the park landscape to create attractive and seamless transitions.
- Provide opportunities for both upstream and downstream river views.
- Honor existing landmarks and community spaces, such as the Roy Montelongo Scenic Overlook, a commemorative public art installation.



The tranquility of the peninsula should be preserved.



This structure commemorates the Tejano music legend, Roy Montelongo.

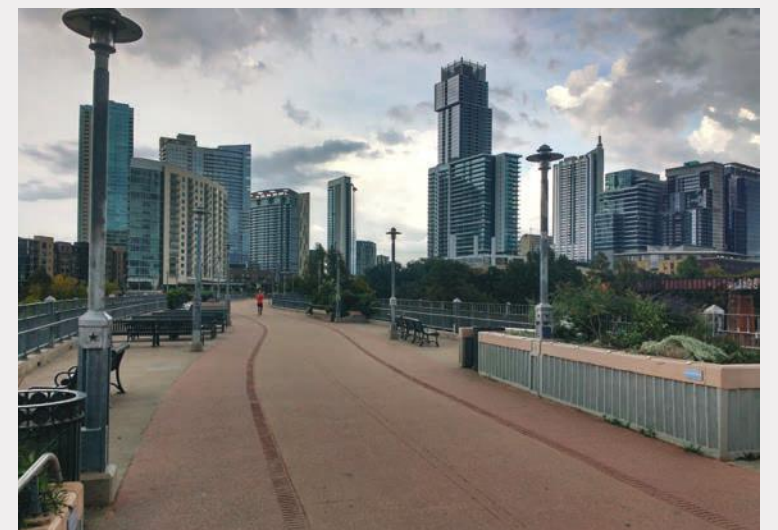
## ④ FRIENDLY & ENGAGING DECK SPACE:

Design the bridge deck, furnishings, connecting trails and spaces to create a great user experience.

- A variety of seating should be available on the bridge deck to provide places for rest, relaxation, and enjoyment of views.
- Guardrails should minimally obstruct views and allow airflow.
- Provide shade on portions of the bridge to increase comfort.
- Bridge "finishings" (guardrails, paving, lighting, etc.) should be carefully detailed using durable materials.



The boardwalk has several shade structures and overlooks.

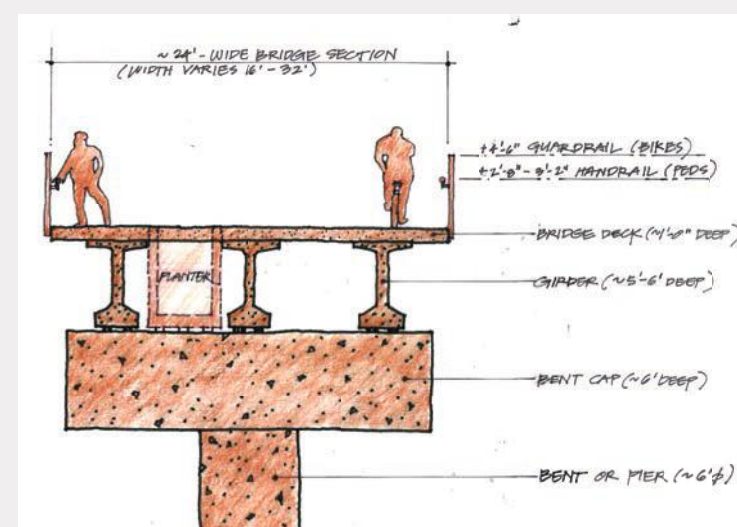


Pfluger Bridge hosts a variety of informal activities.

## ⑤ COST-EFFECTIVE STRUCTURE:

Design the bridge structure to be as time and cost-efficient as possible, allowing for a strong focus on the user experience at the deck level.

- Use simple, cost-effective structural elements.
- Provide elements that enhance function and appearance at a reasonable cost, such as seating, shade, specialty paving, attractive guardrails and plantings.



Standard, precast structural components will be used for the bridge structure



Using a simpler structure still allows for a high quality deck experience

## ⑥ STEWARDSHIP:

With partners, develop and sustain an operations and maintenance program to ensure a high-quality level of care for the bridge and adjacent parkland.

- Identify maintenance partners to ensure maintenance of enhanced level of landscape treatments and shade structures.
- Identify partners to program events, performances, outdoor education, etc.



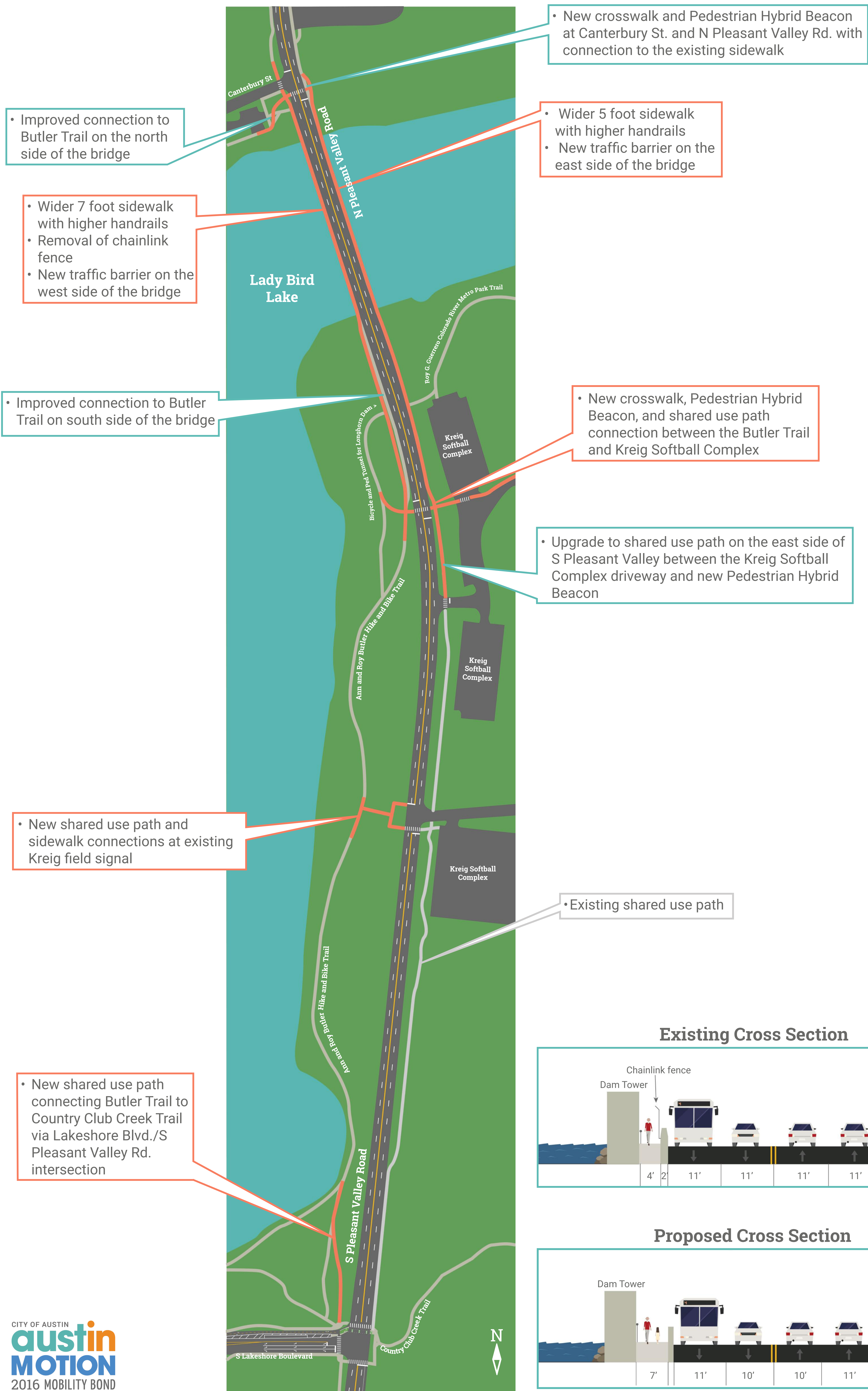
The Congress Avenue trail underpass and overlook was sponsored by The Trail Foundation.



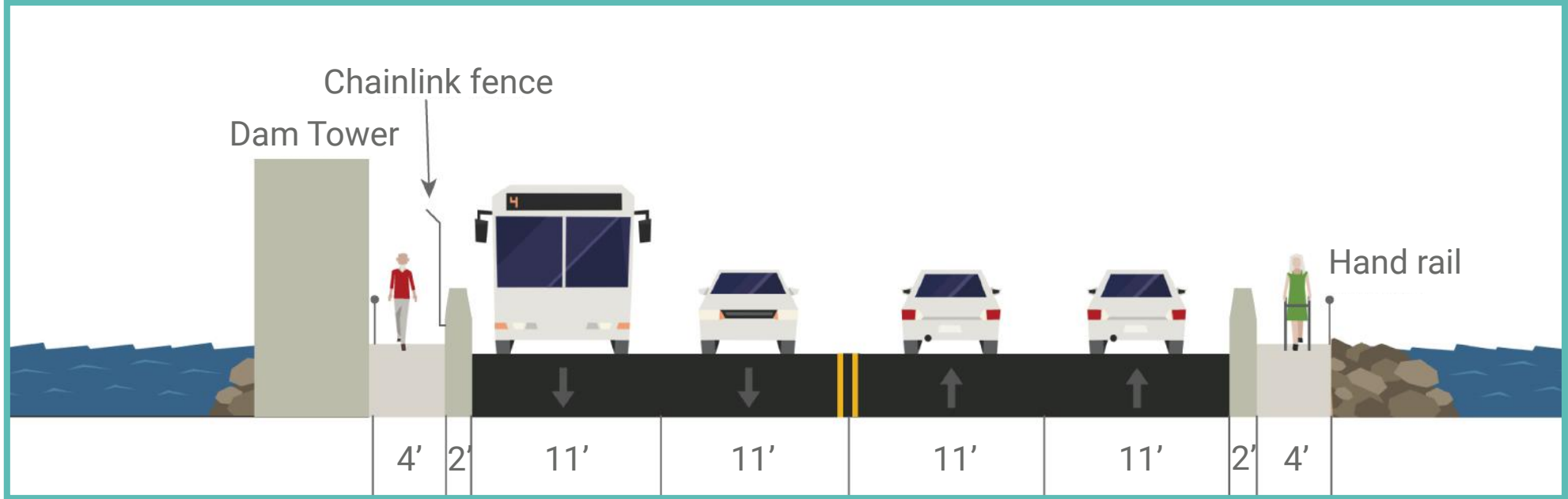
The Boardwalk's landing plaza includes partner recognition bricks and signs.



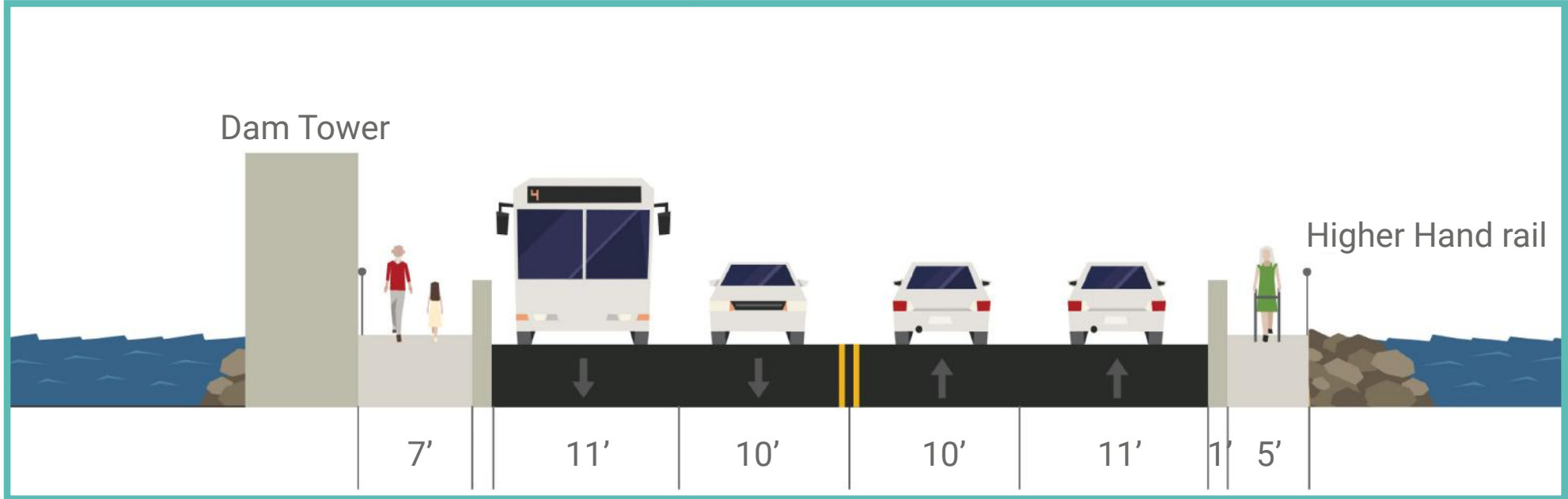
# PROPOSED INTERIM IMPROVEMENTS



Existing Cross Section



Proposed Cross Section



\*Scope of proposed interim improvements is preliminary and subject to change



# FUNDING AND TIMELINE



Funding for the Preliminary Engineering Report and early stages of design is available from several City of Austin funding sources, including the 2016 Mobility Bond Bikeways program



Funding for the proposed interim improvements is available from the 2016 Mobility Bond Bikeways program



Funding for bridge construction will still need to be identified

## Preliminary Engineering Report (PER) Timeline

